



CITY OF HAYWARD AGENDA REPORT

AGENDA DATE January 20, 1998

AGENDA ITEM _____

WORK SESSION ITEM WS# 2

TO: Mayor and City Council

FROM: Director of Community and Economic Development

**SUBJECT: DEVELOPMENT GUIDELINES FOR THE SOUTH OF ROUTE
92 OLIVER AND WEBER PROPERTIES**

Recommendation:

It is recommended that City Council review and comment on the Development Guidelines for the South of 92 Oliver and Weber properties.

Background:

In the October 28, 1997 worksession, staff and consultants reviewed the draft Specific Plan and draft Environmental Impact Report for the South of 92 project with Council. In addition to preparation of the Specific Plan and EIR, the Specific Plan process includes the preparation of development guidelines for the design, siting, construction and maintenance of the land uses identified in the Specific Plan. The Guidelines presented at worksession today are a draft for Council review and comment.

The Guidelines are used, in conjunction with City codes, standards and ordinances, by the Master Developer, to design and build the major elements of the Plan. The guidelines are also used by the City in evaluating applications for site development in the Plan area.

Guidelines Overview

The Development Guidelines provide a consistent set of standards for all phases of development. The Guidelines are proposed to be adopted when action is taken on the Specific Plan to ensure that the development called for in the Plan is attractive, distinctive, safe and enduring. They focus primarily on all of the public elements of a project (the streets, parks, paths/trails, open space) and upon those aspects of specific site development that have the greatest effect on the quality of the public elements, including street landscaping, driveways, parking, building setbacks, height and mass requirements, and site landscaping.

The Development Guidelines are divided into sections which focus on the business park/light manufacturing zones, the residential development, overall landscape design, the sports park, walls and fencing, lighting, and signage.

Business Park / Light Manufacturing

For the Oliver East and Weber parcels, the Guidelines describe restrictions on use within the business park/light manufacturing zones (pages 2-4 through 2-8). The landscape master plans section illustrates the features and intent of the landscape concept (pages 2-8 through 2-10). Within the business park/light manufacturing and residential development, the guidelines are divided into three groups with specific treatments to unify the South of Route 92 Plan Area:

- ◆ Public Corridor -- which includes the vehicular travel and turning lanes, medians, bike lanes, curbs, gutters, traffic signage and signals. All of these improvements will be built by the Master Developer to City standard and, except for the medians, will be maintained by the City.
- ◆ Public Service Easement (PSE) -- which extends from the back of the curb to the edge of each Development Site. It includes the medians, sidewalks, off-street bike paths, plan area decorative street lighting, street furniture, project-related signage, bus stops, easement for trenched utilities, primary and secondary gateways into the Plan Area, PSE landscaping, grading, and irrigation. These improvements will be built by the Master Developer and maintained by the Master Developer and, when the Plan Area is built-out, by the Commercial Property Owners Association.
- ◆ Development Site -- which encompasses all exterior elements within an individual parcel and excludes the adjoining PSE. There is more design flexibility within the individual parcel. However, owners must still meet all of the siting and design requirements of these Guidelines. The parcel owner will construct, install and maintain the parking lots, landscaping, irrigation, site lighting, regulatory and directional signage, service area screening and side and rear yard treatment.

To make it easier for the reader to see how the Public Corridor and the Streetscape inter-relate, Pages 2-13 through 2-26 depict the street cross-sections with each of the appropriate landscape cross-sections for the Oliver East parcel business park/light manufacturing. Pages 2-27 and 2-28 depict the street cross-section for the Weber property light manufacturing. The landscape cross-section for this type of street is on page 2-17 since streets D, E, and F have the same type of landscape treatment.

The landscape character for the public spaces of the business park/light manufacturing zones is established by--

- ◆ carefully engineered road rights-of-way;
- ◆ broad landscape-enhanced public service easements on all public roadways;
- ◆ the requirement that entry landscaping and monumentation be designed to create a desirable business park and residential arrival sequence;
- ◆ requirements for the screening of parking so it is not visible from the street.

The Guidelines introduce the concept of Floor Area Ratios (FARs) as a way to control the amount of development on a given site. A very complicated version of this concept was used in Hayward about 20 years ago in the redevelopment area but fell into disuse and was discontinued more than 10 years ago. In the Guidelines, FAR is defined as "the gross square footage of all buildings...on a parcel divided by the gross square footage of the parcel. The resulting percentage must not exceed the maximum FAR established for each type of land use within the allowable building height, setback and site landscape coverage requirements. Pages 2-43 and 2-44 illustrate the FAR concept. FARs are commonly employed by localities for this type of development.

The character of the built environment is created through the establishment of FARs to control the amount of site development while creating the type of development which encourages major business park tenants. Other site development standards such as front, side and rear yard setbacks are included that ensure high quality development will occur that is compatible with surrounding uses.

Residential Development

The Residential Development on Oliver West is characterized by a memorable arrival sequence via the landscaped overpass which provides a vista of the residential area and Wildlife Conservation Board property beyond. Residents arrive at a large landscaped roundabout with a pedestrian parkway and a five acre neighborhood park. Within the neighborhood is a 25-foot wide pedestrian parkway which can be used by residents to walk between the area's two neighborhood parks. Streets will be lined by street trees and will have sidewalks on both sides. There will be an open space buffer with natural vegetation and running water on the western residential edge, creating views towards the Bay. Within the Plan Area, walls and fencing are kept to a minimum and are only prescribed when necessary to protect one use from another.

Mayor and City Council
Development Guidelines for South of 92
January 20, 1998

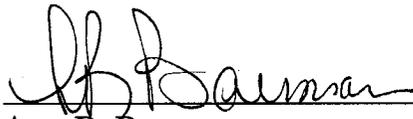
Sports Park

The Sports Park is briefly described in concept here. HARD is currently working on the park design.

Next Steps

This worksession is an opportunity for Council to obtain an overview of the Guidelines and discuss areas of interest. It is recognized that there may not have been sufficient time for Council to review the Guidelines in detail; therefore, in February 1998, when Council reviews the final Specific Plan and EIR, these guidelines will be reviewed within that context. Additional comments, questions or issues can be addressed at that time.

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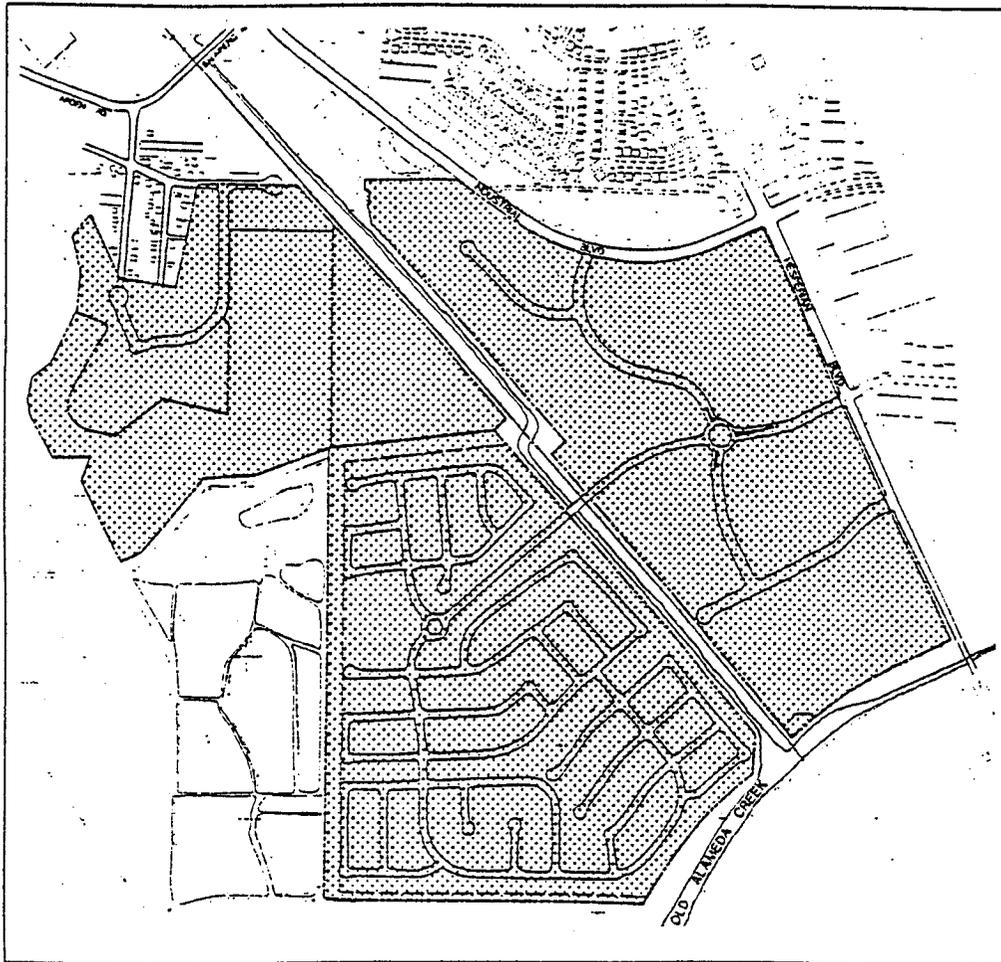


Jesús Armas
City Manager

Attachment: Development Guidelines

SOUTH OF ROUTE 92 OLIVER / WEBER PROPERTIES

HAYWARD, CALIFORNIA



DEVELOPMENT GUIDELINES

DRAFT

SOUTH OF ROUTE 92
OLIVER / WEBER PROPERTIES
HAYWARD, CALIFORNIA

DEVELOPMENT GUIDELINES

DRAFT, January 15, 1998

PREPARED FOR:
THE CITY OF HAYWARD

PREPARED BY:
PBR, INC.

WITH SUPPORT FROM REAL ESTATE PLANNING STRATEGIES AND
THE CITY OF HAYWARD

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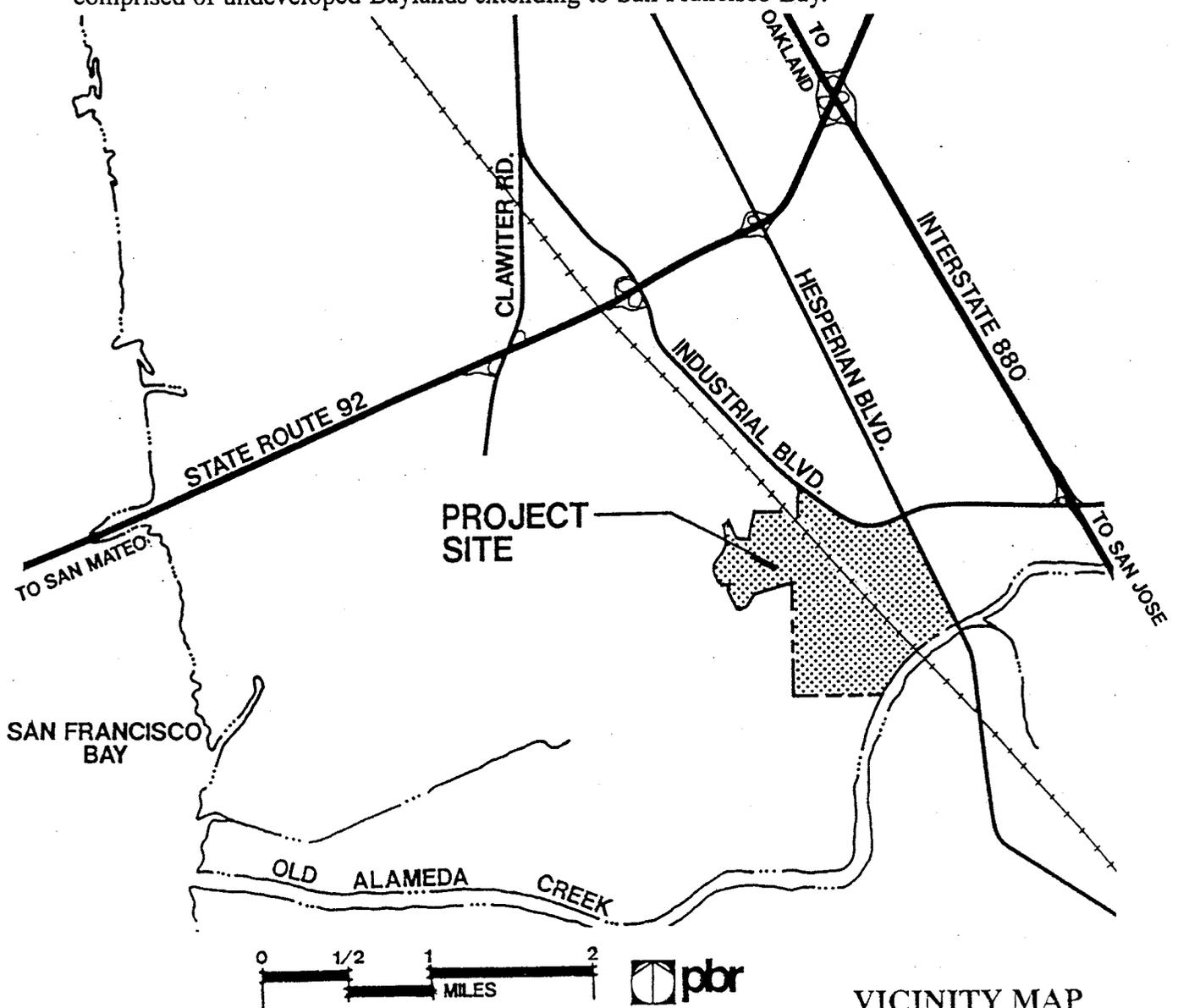
SECTION 1 - PROJECT OVERVIEW

1.0 PROJECT OVERVIEW

1.1 INTRODUCTION

The South of Route 92 Oliver/Weber Properties project is a new mixed-use development consisting of a business park, high quality single family housing, a 25 acre sports park and light manufacturing. This combination of uses is intended to minimize traffic congestion by locating housing near jobs and recreation activities.

South of Route 92 Oliver/Weber Properties is generally defined by existing industrial development within the City's Industrial corridor on Industrial Blvd. to the north and Hesperian Blvd. to the east. Old Alameda Creek, The Kaiser Medical offices and clinic and the municipality of Union City are to the south. The westerly boundary is formed by the Baumberg Tract which is now owned by the State of California Wildlife Conservation Board and is comprised of undeveloped Baylands extending to San Francisco Bay.



VICINITY MAP

Exhibit 1-A

Project Overview

1.2 PROJECT DESCRIPTION

The Plan Area includes land owned by the Oliver Trust, Mr. John Weber, the City of Hayward and the Alameda County Flood Control and Water Conservation District. The Oliver Properties are bisected by the Southern Pacific/Union Pacific railroad tracks on a northwest southeast axis. The land uses proposed for the east half (Oliver East) consist primarily of business park, light manufacturing and a 25 acre sports park adjacent to Hesperian Boulevard. Oliver East will also contain a 3.5 - 5 acre commercial/retail site adjacent to the primary project gateway. The west portion (Oliver West) contains single family detached homes and two neighborhood parks. The residential neighborhoods have access to a pedestrian and bicycle trail link to the Bay Trail system.

To the northwest, the Weber property is planned as light manufacturing, wetlands and salt marsh harvest mouse habitat. From the primary project gateway on Hesperian Boulevard, a new community collector roadway, Street A, traverses westerly over the railroad tracks and terminates at the residential neighborhoods. The Weber property is served by a street extending from Old Arden Road. The homes and businesses are buffered from adjacent wetland and habitat areas by walls, roadways, significant elevation differences and open space corridors containing running water.

1.3 ZONING PLAN

Development within the South of Route 92 Plan Area Business Park shall conform to the zoning established on Exhibit 1-B: Plan Area Zoning Plan. Specific uses allowed by zoning are set forth in Section 2.2 and 3.2.

The Zoning Plans illustrate the variety and locations of permitted land uses including Business Park, Light Manufacturing and Commercial/Retail, Residential, Open Space, Wetlands and Habitat.

1.4 LANDSCAPE MASTER PLAN

The overall landscape concept for the plan area is to provide attractive public spaces throughout the mixed use development. Ample landscaping is required along all streets to ensure the appearance of a high quality development. Special landscape emphasis is placed on the primary gateways. Distinctive landscape treatment harking back to the plan area's history of flower production, is also required. Landscaped roundabouts and a landscaped neighborhood park are in locations that can be enjoyed by residents and visitors alike. Similarly, a landscaped parkway forms one edge of the primary residential collector street.

1.5 DEVELOPMENT GUIDELINES ROLE

- A. The purpose of the Development Guidelines is to foster variety and design creativity by parcel developers within the overall context of the community theme. It is intended to guide site planning, product design and landscape development on all parcels within the community.

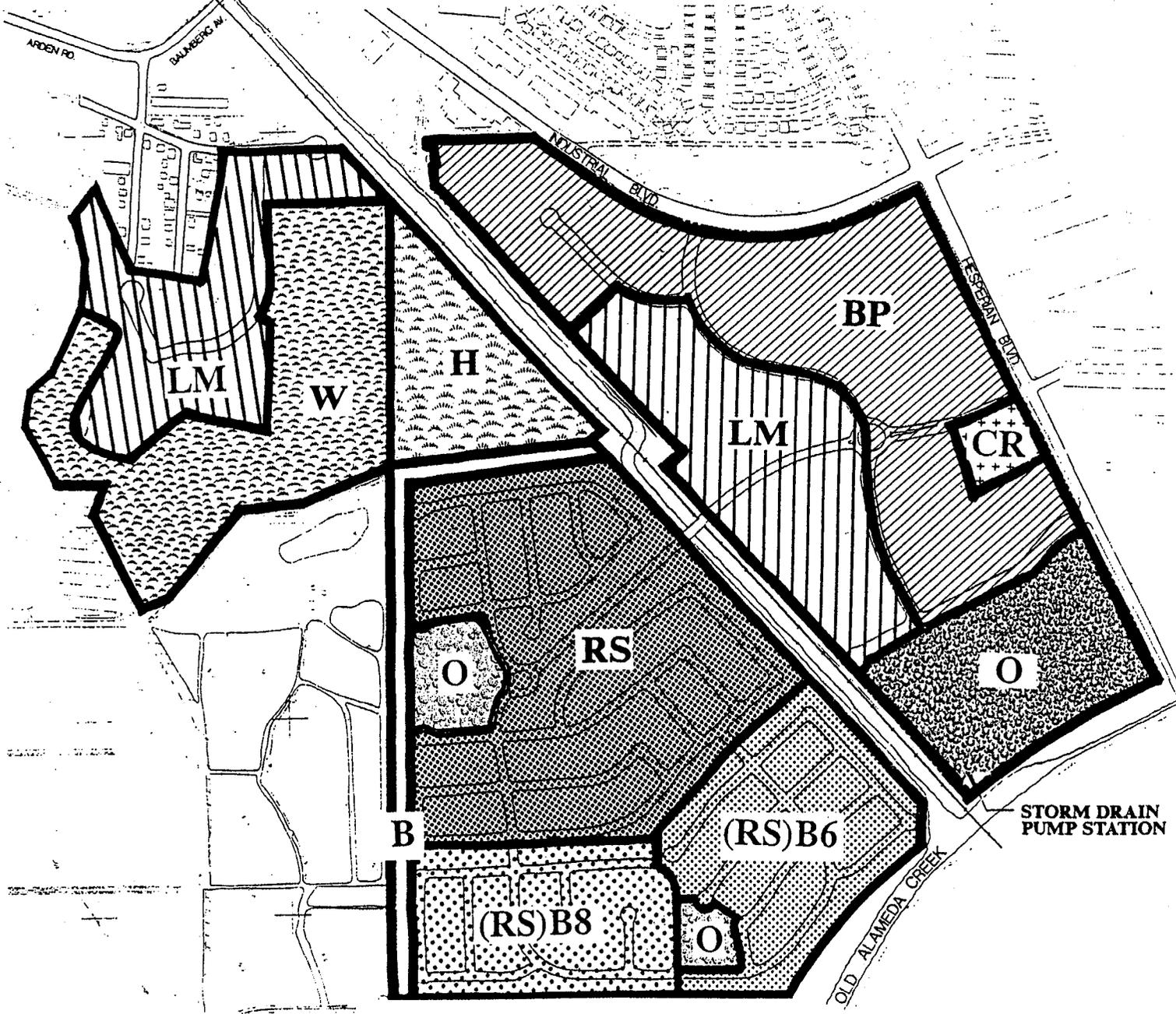
- B. These Development Guidelines supplement and refine the City's Design Standards for this Plan Area. They do not supersede any existing applicable codes or ordinances. Any requests for variances or exceptions to the laws and regulations of the City must be made to the City according to established procedures.
- C. Responsibility for meeting the Development Guidelines lies with the individual parcel developers. The Design Review process is described in detail in Section 8.0 of this document.

**TABLE 1-A
SUMMARY OF LAND USES**

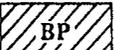
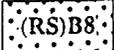
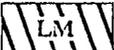
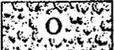
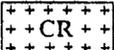
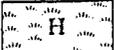
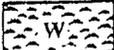
	Business Park	Residential	Light Manufacturing	Retail	Parks	Roads	Landscape/ Buffer/ Wetlands	Total Acres
Oliver East	42.1	--	27.6	3.5	23*	10.1	2.0	108.3
Oliver West	--	81.5	--	--	7.5	26.1	15.4	130.5
City of Hayward	12.0	--	--	--	--	0.2	---	12.2
Weber	--	--	21.5	--	--	2.0	57.0	80.5
ACFCWCD	--	--	--	--	2.0*	--	--	2.0
Total	54.1	81.5	49.1	3.5	32.5	38.4	74.4	333.5

* Sports Park

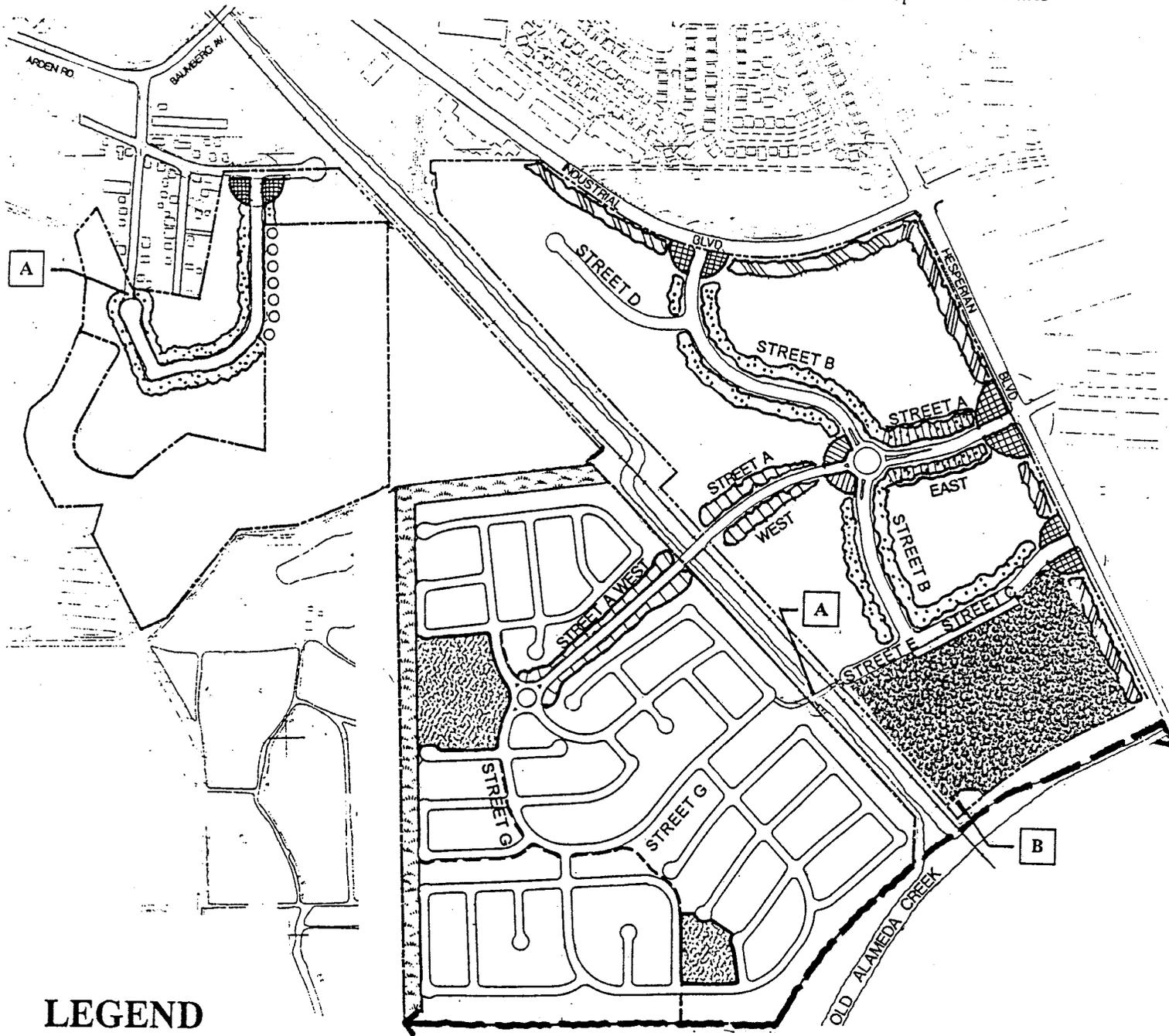
Source: Ruggeri-Jensen & Associates, Land Use Plan, South of Route 92 Specific Plan, June 27, 1997.
Economic and Planning Systems, Chapter X, Financing and Implementation, South of Route 92 Specific Plan, Draft, August 15, 1997



LEGEND

	BP BUSINESS PARK		(RS)B8 RESIDENTIAL-SINGLE FAMILY 8,000 SQ. FT. LOTS		B OPEN SPACE-BUFFER ZONE
	LM LIGHT MANUFACTURING/RESEARCH AND DEVELOPMENT		O OPEN SPACE-NEIGHBORHOOD PARK		
	CR COMMERCIAL/RETAIL		O OPEN SPACE-SPORTS PARK		
	RS RESIDENTIAL-SINGLE FAMILY 5,000 SQ. FT. LOTS		H HABITAT		
	(RS)B6 RESIDENTIAL-SINGLE FAMILY 6,000 SQ. FT. LOTS		W WETLANDS		

PLAN AREA ZONING PLAN



LEGEND

- | | | | | | |
|--|--|--|-------------------------------------|--|--|
| | MAIN PROJECT GATEWAY LANDSCAPING | | OPEN SPACE- NEIGHBORHOOD PARK | | REGIONAL ARTERIAL ROADWAY PROJECT EDGE TREATMENT |
| | SECONDARY PROJECT GATEWAYS LANDSCAPING | | SPORTS PARK | | MAIN PROJECT GATEWAY ROADWAY LANDSCAPING |
| | RESIDENTIAL ENTRY LANDSCAPING | | BUFFER ZONE | | NEIGHBORHOOD TRAIL |
| | WALL WITH ROW OF TALL SHRUBS AND VINES | | OVERPASS LANDSCAPING TO RESIDENTIAL | | A EMERGENCY VEHICLE ACCESS |
| | | | COLLECTOR ROADWAY LANDSCAPING | | B STORM DRAIN PUMP STATION |

PLAN AREA LANDSCAPE MASTER PLAN

Exhibit 1-C

Project Overview

SECTION 2 - BUSINESS PARK/
LIGHT MANUFACTURING
DEVELOPMENT

2.0 BUSINESS PARK/LIGHT MANUFACTURING DEVELOPMENT

2.1 BUSINESS PARK/LIGHT MANUFACTURING ZONING PLAN

Development within the South of Route 92 Plan Area Business Park shall conform to the zoning established on Exhibit 2-A and 2-B: Zoning Plans. Specific uses allowed by zoning are set forth in Section 2.2.

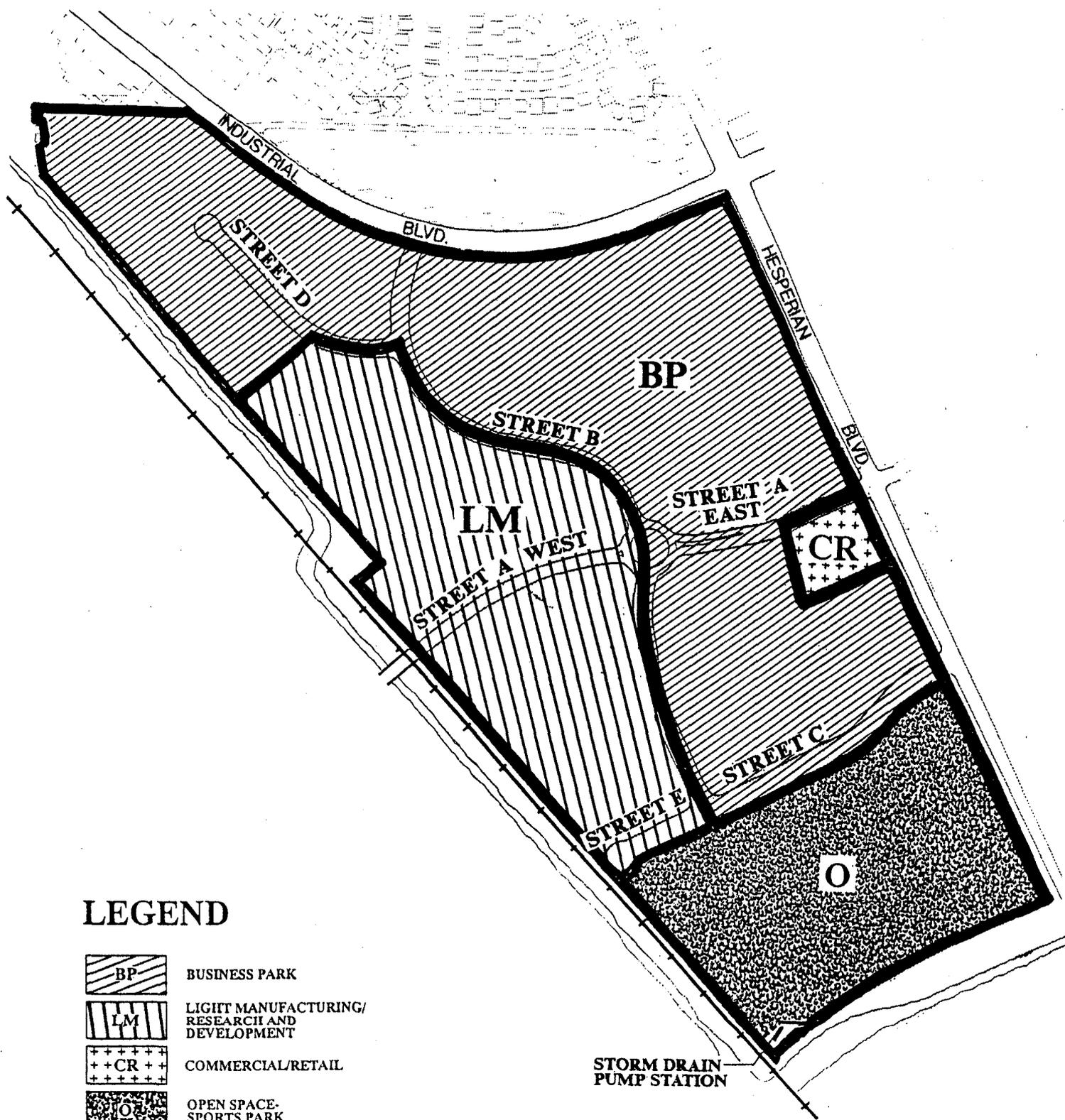
The Zoning Plans illustrate the variety and locations of permitted land uses including Business Park/Light Manufacturing and Commercial/Retail, Residential, Parks, Open Space, Wetlands and Habitat.

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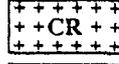
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* Sports Park

Source: Ruggeri-Jensen & Associates, Land Use Plan, South of Route 92 Specific Plan, June 27, 1997. Economic and Planning Systems, Chapter X, Financing and Implementation, South of Route 92 Specific Plan, Draft, August 15, 1997



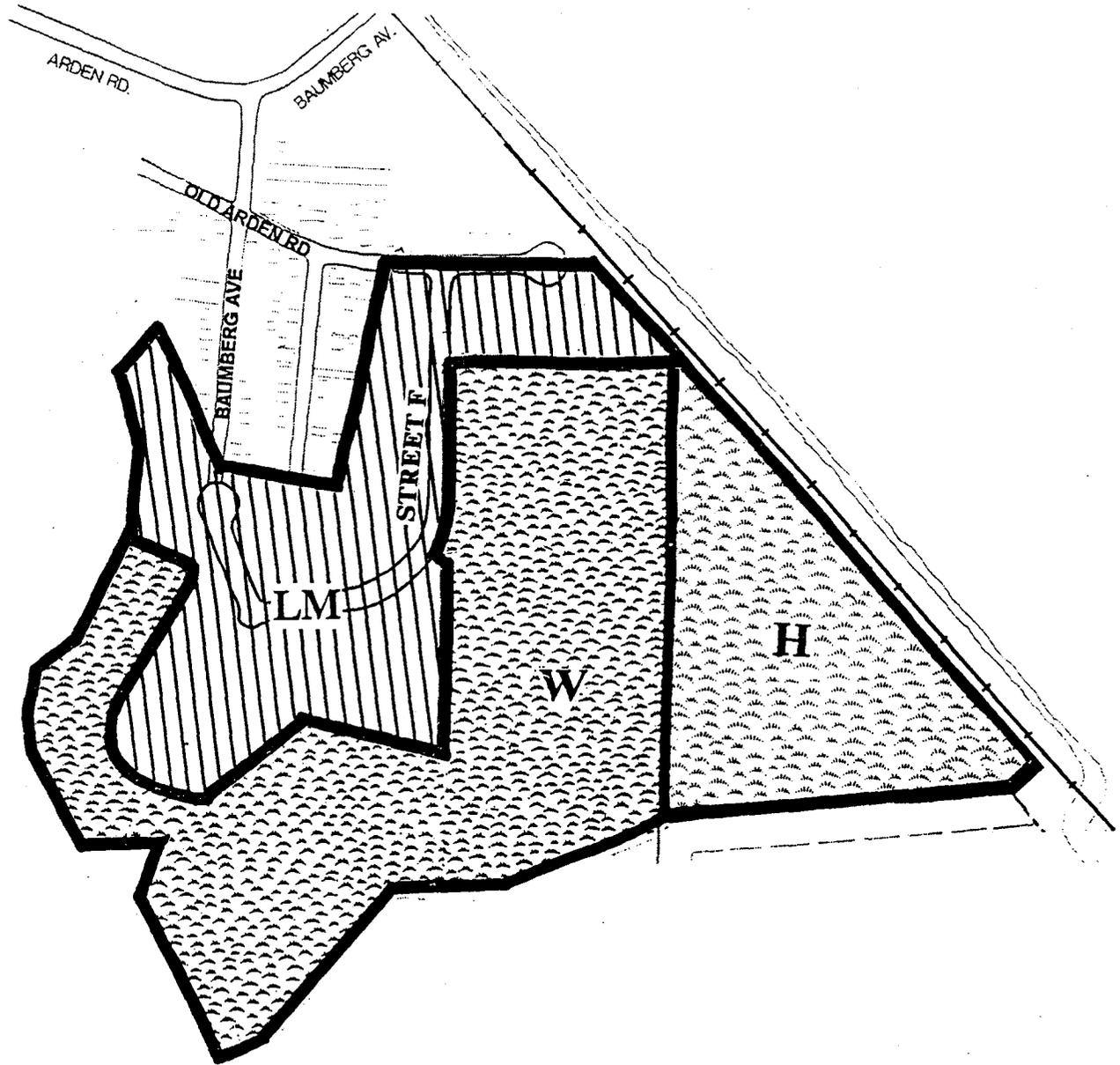
LEGEND

-  BUSINESS PARK
-  LIGHT MANUFACTURING/
RESEARCH AND
DEVELOPMENT
-  COMMERCIAL/RETAIL
-  OPEN SPACE-
SPORTS PARK

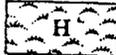
OLIVER EAST BUSINESS PARK/LIGHT MANUFACTURING ZONING PLAN

Exhibit 2-A

Business Park/Light Manufacturing Development



LEGEND

-  LIGHT MANUFACTURING
-  WETLANDS
-  HABITAT



WEBER LIGHT MANUFACTURING ZONING PLAN

Exhibit 2-B

Business Park/Light Manufacturing Development

2.2 RESTRICTIONS ON USE

- A. The use of each Business Park or Light Manufacturing parcel shall be in compliance with the zones as illustrated on the Zoning Plan. Permitted uses are set forth by the planning district for the referenced parcels, and such permitted uses shall govern every portion of such parcel, even if the parcel is subdivided into smaller lots.
- B. The types of uses that will occupy the site may not involve noxious odors, smoke, or other uses that negatively impact air quality either beyond acceptable standards of the Bay Area Air Quality Management District or that would negatively impact the adjacent residential uses.

Proposed businesses which would use substances classified as hazardous materials will be

- C. reviewed on an individual basis and approved only if compatible with surrounding businesses and residences. Based on the type and amount of hazardous materials used, the business must file a Hazardous Materials Plan with the Fire Department. An administrative use permit is required if the Fire Department determines that the type and amount of hazardous materials meet thresholds that would require additional regulations.

The following uses are permitted; some are subject to Use Permit requirements, as noted by an asterisk (*).

• Business Park ("BP")

- (a) Administrative, executive and business offices
- (b) Business service offices, including employment agencies, accountants, notaries, stenographic, addressing, computing and related services
- (c) Business consultant offices
- (d) Design professions offices (engineering, architectural, drafting, etc.)
- (e) Research, development, analytical and scientific offices
- (f) Manufacturers' representatives and sales offices
- (g) Headquarters or region-wide finance, insurance and real estate offices
- (h) Medical and dental clinics that provide services to workers in the light manufacturing areas, and nearby residents
- (i) Travel agencies
- (j) Publishing
- (k) Any other use determined by the Planning Director to be substantially similar to any of the above permitted uses.

• Light Manufacturing, Planning/Research and Development ("LM")

- (a) Manufacturing and assembly of business machines, including electronic data processing equipment, accounting machines, calculators and related equipment. Manufacturing and assembly of computer hardware and software, communications, testing equipment, and furniture, including semiconductor design and prototype production.
- (b) Manufacturing and assembly of electrical supplies, such as coils, condensers, crystal holders, insulation, lamps, switches and wire cable assembly, provided no noxious or offensive fumes or odors are produced

- (c) Manufacturing of scientific, medical, dental and drafting instruments, orthopedic and medical appliances, optical goods, watches and clocks, electronics equipment, precision instruments, musical instruments and cameras and photographic equipment except film
- (d) Manufacturing or combining processes of pharmaceutical products, provided no noxious or offensive fumes or odors are produced.
- (e) Manufacturing or combining processes of biological products, provided no noxious or offensive fumes or odors are produced.
- (f) Manufacturing and assembly of clothing
- (g) Manufacturing and assembly of televisions and radios, including parts and components
- (h) Assembly of electric appliances such as lighting fixtures, irons, fans, toasters and electric toys, but not including refrigerators, washing machines, dryers, dishwashers and similar home appliances.
- (i) Assembly of electrical equipment such as radio and television receivers, phonographs and home motion picture equipment, but not including electrical machinery
- * (j) Laboratories, including commercial, testing, research, experimental or other laboratories, including pilot plants
- (k) General office uses (including computer centers) where no office user shall have less than 2,000 square feet of usable space
- * (l) Printing, lithography and engraving
- (m) Publishing
- (n) Light manufacturing support and service facilities to include activities limited to the servicing of products produced on the Parcel or servicing of businesses on the Parcel, such as: repair and maintenance of appliances or component parts, tooling; printers, testing shops, small machine shops, copying, photo engraving, etc.
- * (o) Accessory uses and structures when related to and incidental to a permitted use
- * (p) Manufacturing or combining processes of pharmaceutical products, provided no noxious or offensive fumes or odors are produced
- (q) Sales at wholesale manufacturers' representatives and sales office, or sales to the ultimate consumer of products made to the customer's orders
- (r) Engineering, drafting and design facilities
- * (s) Research and development facilities and such facilities which require area available for laboratories to execute product development. Any research and development use may be operated in conjunction with any allowed light manufacturing use or office use. All typical uses associated with research and development and light manufacturing

for the electronics and semiconductor industries.

- (t) Any research and development use listed above may be operated in conjunction with any allowed light manufacturing use or office use
- * (u) All typical uses associated with research and development and light manufacturing for the electronics and semiconductor industries
- (v) Public utility equipment buildings and public service pumping stations
- (w) Any other use determined by the Planning Director to be substantially similar to any of the above permitted uses.

• **Commercial/Retail District ("CR")**

- (a) Bakeries, including baking for sale on premises only
- (b) Barbers and beauty shops
- (c) Bicycle shops
- (d) Bookstores and libraries
- (e) Carpet, drapery and floor covering stores
- (f) Catering establishments
- (g) Computer and scientific instrument shops
- (h) Delicatessen limited to a maximum of 3,000 square feet
- (i) Drug stores and prescription pharmacies
- (j) Electrical appliance repair and sales
- (k) Depository/Financial institutions (Banks, Savings & Loans, Credit Unions)
- (l) Florists
- (m) Gift shops
- (n) Grocery Stores
- (o) Gymnasiums and health clubs
- (p) Hardware stores
- (q) Hobby shops
- (r) Ice cream stores
- (s) Janitorial services and supply establishments (Retail)
- (t) Laundries and dry cleaners (Retail)
- (u) Locksmiths
- * (v) Medical and dental clinics that provide services to workers in the light manufacturing areas, and nearby residents
- (w) Medical/Orthopedic appliance stores
- (x) Music stores
- (y) Newsstands
- (z) Office supply, furniture and business machine stores
- * (aa) Second Floor Offices, including but not limited to business, professional and administrative offices
- (bb) Optical and optometry shops
- (cc) Paint, glass and wallpaper shops

- (dd) Photographic supply stores
- (ee) Picture framing shops
- (ff) Post offices
- (gg) Reprographic services
- (hh) Commercial realtors and real estate offices
- (ii) Full service restaurants
- (jj) Shoe repair shops
- (kk) Sign painting shops
- (ll) Sporting goods stores including mens and womens shoes and apparel
- (mm) Stationery stores
- (nn) Tailor or dressmaking shops
- (oo) Travel agencies and bureaus
- (pp) Video rentals
- (qq) Watch and clock repair shops
- *(rr) Any other use determined by the Planning Director to be substantially similar to any of the above permitted uses.

- C. **Conduct of Permitted Uses.** All uses shall be performed and carried out entirely within a building designed and constructed for that use. Activities which cannot be carried on within a building may be permitted, but only if the City of Hayward consents in writing to said use and to the location for such activity, and if said use is permitted by the then existing zoning or other applicable land use regulations.
- D. **Emissions.** As determined by the City or other responsible agency, no use shall be permitted on any Parcel which:
- (a) Emits dust, sweepings, dirt, cinders, fumes, odors, radiation, gases or vapors
 - (b) Discharges liquid or solid wastes or other harmful matter into the atmosphere or any stream, river, canal, flood control channel or other body of water, which may adversely affect (i) the health or safety of persons or (ii) the use or enjoyment of property within the Specific Plan Area, or (iii) vegetation within the Specific Plan Area.
 - (c) Discharges waste or any substance or materials of any kind into any public sewer serving the Business Park or any part thereof in violation of any regulations of any public body having jurisdiction
 - (d) Produces intense glare or heat unless such use is performed only within an enclosed or screened area and then only in such a manner that the glare or heat emitted will not be discernable from any property line of said parcel
 - (e) Creates a sound pressure level in violation of any regulation of any public body having jurisdiction
 - (f) Allows the visible emissions of smoke or steam (outside any building), other than the exhausts emitted by motor vehicles or other transportation facilities, in violation of any regulation of any public body having jurisdiction; or motor vehicles or other transportation facilities, in violation of any regulation of any public body having jurisdiction; or

- (g) Creates a ground vibration that is perceptible, without instruments, at any point along any of the property lines of said parcel.
- E. **Animals.** No animals, livestock or poultry of any kind shall be raised, bred or kept upon the property, except for small laboratory animals for biotech research.
- F. **Nuisances.** No use or expansion of any existing use shall be conducted in a manner that is detrimental to or incompatible with adjacent properties so as to create any dangerous, noxious, or otherwise objectionable fire, explosive, or other hazard; noise or vibration; smoke, dust, odor, or other form of air pollution; heat, cold, dampness, electrical, or other disturbance; glare; liquid or solid refuse or wastes; or other substance, condition, or element, which are a nuisance to owners or users of adjoining property or the general public. See Sec. 10-1.4900 in Hayward Zoning Ordinance #96-20.

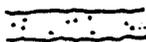
2.3 BUSINESS PARK/LIGHT MANUFACTURING LANDSCAPE MASTER PLANS

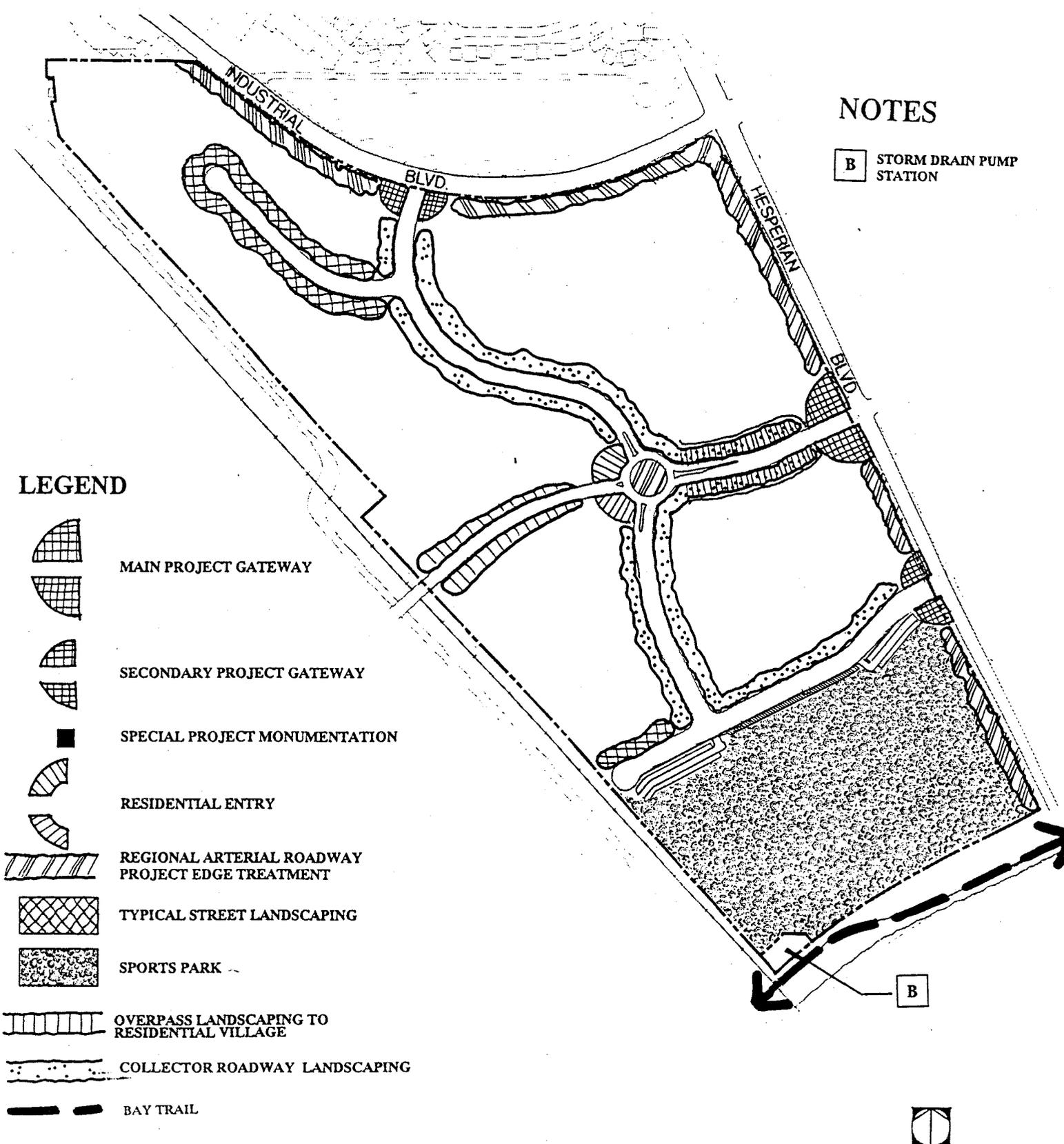
- A. The Landscape Master Plans illustrate the features and intent of the landscape concept. The overall concept for the public rights-of-way of the South of Route 92 Plan Area Business Park/Light Manufacturing Zone is to establish an attractive image for the park based on the historical commercial flower production of the area and includes:
 - 1. Color on the ground plain and the use of flowers to recall the site's historical significance
 - 2. New Community Collector Landscape. Distinctive landscaping for Street A and Street B including tall columnar trees located within a landscaped mounded parkway
 - 3. Special Project Gateways. Special entries along the project's periphery at key locations to denote the project's entrances
 - 4. Street Trees. A hierarchy of trees and shrubs for each street and parkway.
- B. Landscape Area Defined
 - 1. The term "landscape areas" shall refer to all areas within a given parcel not occupied by a structure and not utilized for parking, truck loading, storage, or refuse collection. Landscape areas shall include all walkways, unless otherwise indicated.
 - 2. The minimum percentage of gross parcel area required to be landscaped is established for each land use and listed in Section 2.6.2.
 - 3. The minimum landscaping requirements on each parcel include, but are not limited to, the Public Services Easement (PSE), the landscape requirements adjacent to the front and side of each building, and the parking lot landscape.

NOTES

B STORM DRAIN PUMP STATION

LEGEND

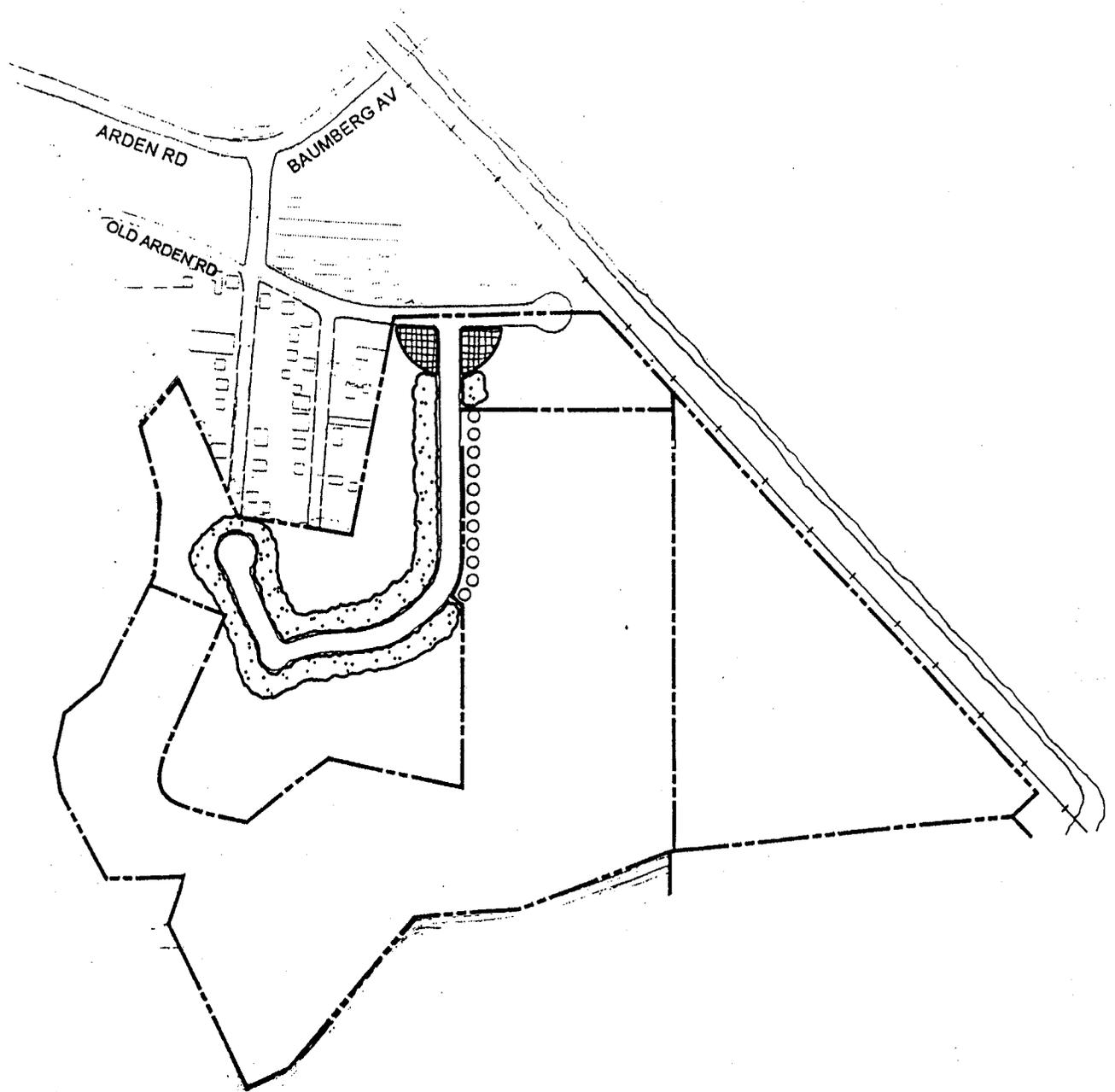
-  MAIN PROJECT GATEWAY
-  SECONDARY PROJECT GATEWAY
-  SPECIAL PROJECT MONUMENTATION
-  RESIDENTIAL ENTRY
-  REGIONAL ARTERIAL ROADWAY PROJECT EDGE TREATMENT
-  TYPICAL STREET LANDSCAPING
-  SPORTS PARK
-  OVERPASS LANDSCAPING TO RESIDENTIAL VILLAGE
-  COLLECTOR ROADWAY LANDSCAPING
-  BAY TRAIL



OLIVER EAST BUSINESS PARK/LIGHT MANUFACTURING LANDSCAPE MASTER PLAN

Exhibit 2-C

Business Park/Light Manufacturing Development



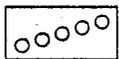
LEGEND



SECONDARY PROJECT GATEWAY



COLLECTOR ROADWAY



WALL WITH ROW OF TALL SHRUBS AND VINES



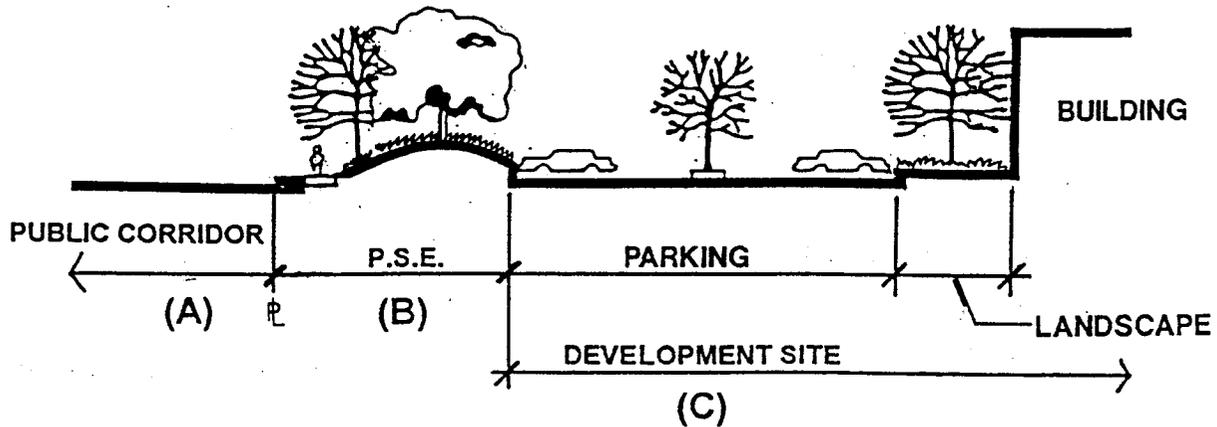
WEBER LIGHT MANUFACTURING LANDSCAPE MASTER PLAN

Exhibit 2-D

Business Park/Light Manufacturing Development

2.4 DEFINITIONS

The Business Park/Light Manufacturing portions of the South of Route 92 Plan Area are located on the Oliver East and Weber parcels. For the purposes of these guidelines, these portions consist of three distinct areas: the PUBLIC CORRIDOR, the PUBLIC SERVICE EASEMENT ("PSE") and the DEVELOPMENT SITE. In general, the Public Corridor and PSE improvements established in these guidelines have been designed with specific treatments to unify the South of Route 92 Plan Area. Design and construction on each development site may be more individualized. The location, definitions and responsibility for the construction and maintenance for the three areas are illustrated in the diagram below and discussed in sequence below.



- A. The Public Corridor within the Plan Area is shown in the street sections on tops of the following pages. It extends from the property line at back of curb to the opposing property line at back of curb. The Master Developer will construct vehicular travel lanes, turning lanes, medians, bike lanes, curbs, gutters, traffic signage, signals and typical street lighting . All of the preceding Public Corridor elements except the medians will be maintained by the City.
- B. The Public Service Easement extends from the property line at the back of curb to the edge of each Development Site. It is shown as landscaped on the bottom of the following pages. In the event that a site is developed with front yard parking, the PSE shall represent the minimum landscape setback to the parking. An additional 15' of landscaping shall be constructed between the parking area(s) and the front of the building structure. The PSE may be 33' or 40' wide. The contents of the PSE and the responsibility for construction, maintenance/repair are as follows:
 1. Medians, sidewalks, off-street bike paths, plan area decorative street lighting, street furniture, project-related signage, bus stops. Bus stops may include a concrete pad, benches, shelters and lighting. These elements will be constructed and maintained by the Master Developer, except for street furniture which the City will maintain.
 2. An easement for trenched utilities. The easement is to be maintained by the Master Developer and the utilities are to be maintained by the utility providers.
 3. One primary and three secondary gateways including walls, theme structures, lighting, signage, landscaping and irrigation. The Master Developer will maintain these elements.

4. PSE landscaping, grading, irrigation. These elements will be constructed and maintained by the individual parcel owner.
 5. All building monument signage including walls, landscaping, irrigation and specialty lighting, individual project driveways, and paths. These will be constructed and maintained by the individual parcel owner.
- C. The Development Site encompasses all exterior elements within an individual parcel, and excludes the adjoining PSE. This area will be subject to the needs of each parcel owner and is separated from the public domain by the PSE. Thus, more design flexibility is required and acceptable within each parcel. The parcel owner will construct, install, maintain and repair or replace as necessary all elements within the Development Site, typically including: parking lots, landscaping, irrigation, site lighting, regulatory and directional signing, service area screening and side and rear yard treatment.
- D. The Master Developer will create either assessment districts or landscape and lighting districts to fund maintenance of Master Developer obligations. A commercial property owners association will be created to carry out maintenance when Master Developer completes the project.

2.5 PUBLIC CORRIDOR

2.5.1 ROADWAYS AND BICYCLE TRAILS

A. Arterials

Hesperian and Industrial Boulevards adjoin the plan area. Both are being improved so that the plan area can be adequately served without impacting current travel conditions. Additional lanes will be added and new signalization will be provided.

B. Community Collector

Street A provides a convenient means to traverse the Oliver East and West sites and access the Business Park and Light Manufacturing uses. It is the primary entry route to the residential neighborhoods. In keeping with this roadway's role, an enriched landscape treatment is envisioned which includes a coordinated scheme within all development areas.

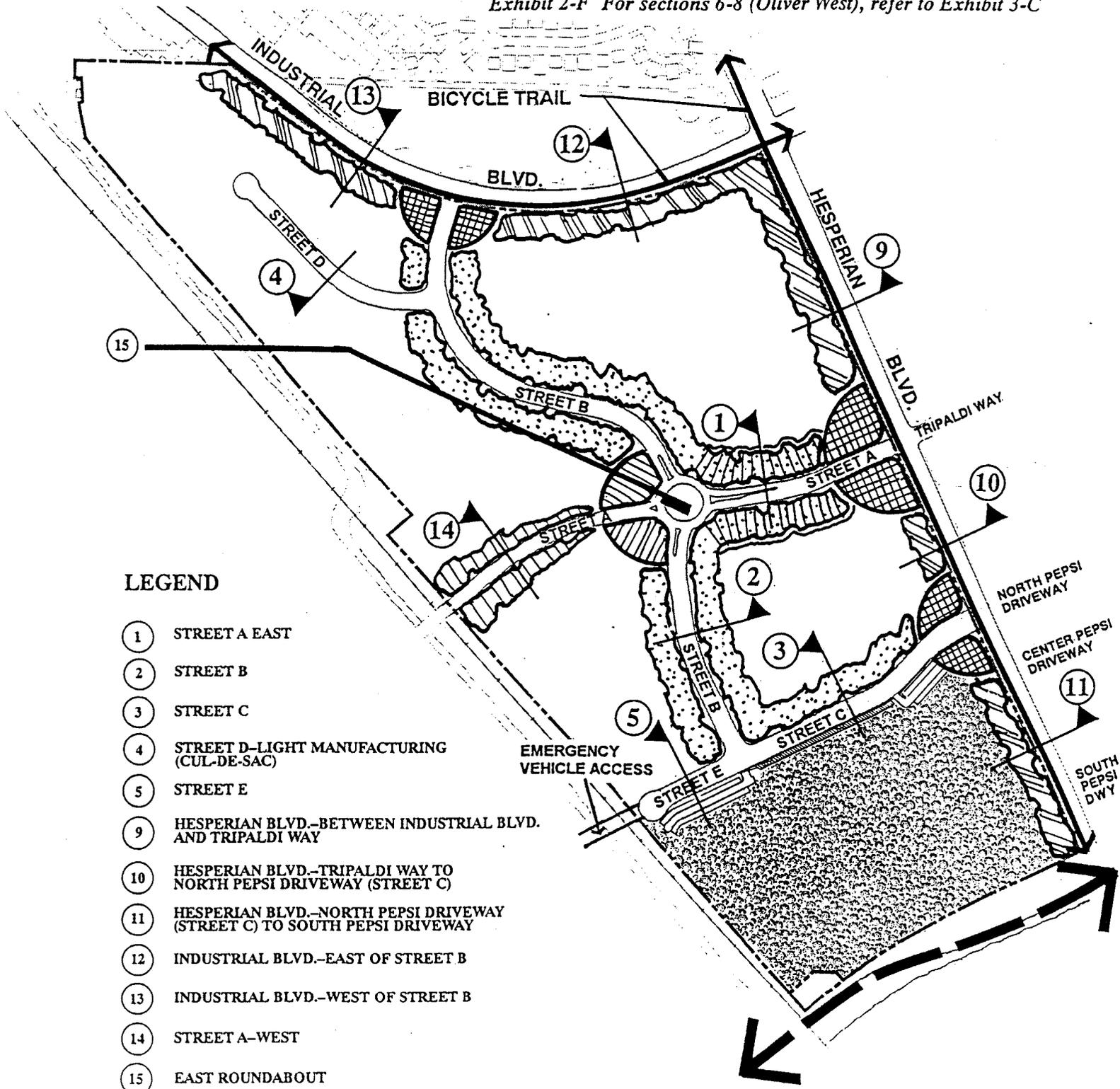
C. Local Streets

Local streets within the Business Park/Light Manufacturing Area shall meet city design standards and convey the landscape scheme. Local streets may be curvilinear ending in cul-de-sacs or loops or traditional grids depending on the parcelization pattern. Sidewalks shall be on both sides of all streets.

D. Bicycle Trails

Class II bicycle trails exist along Hesperian Blvd. and Industrial Blvd. On Industrial Blvd. and on Hesperian Blvd. north of the intersection at Street C, meandering paths, suitable for recreational bike use and pedestrians are part of the streetscape area. South of Street C on Hesperian Blvd., a 10' wide path is to be provided along the edge of the sports park.

Note: The following street sections do not follow in exact numerical sequence. For remaining street section 4 (Weber), refer to Exhibit 2-F For sections 6-8 (Oliver West), refer to Exhibit 3-C

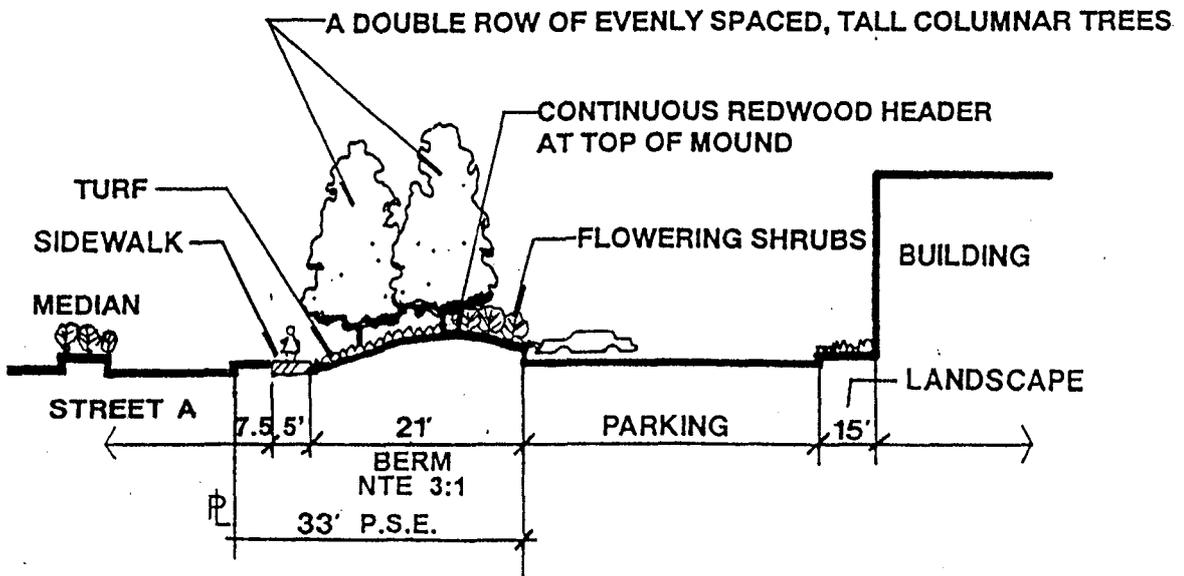
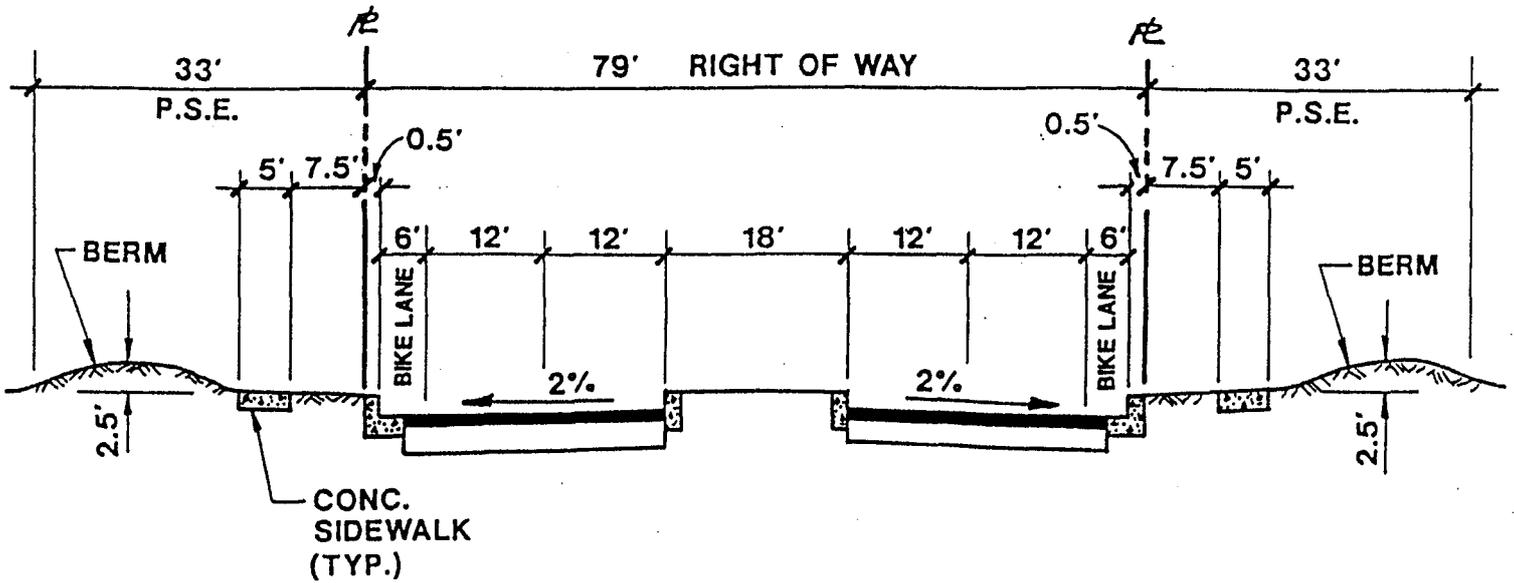


**OLIVER EAST BUSINESS PARK/LIGHT MANUFACTURING
PUBLIC CORRIDOR/PSE CROSS-SECTION KEY**

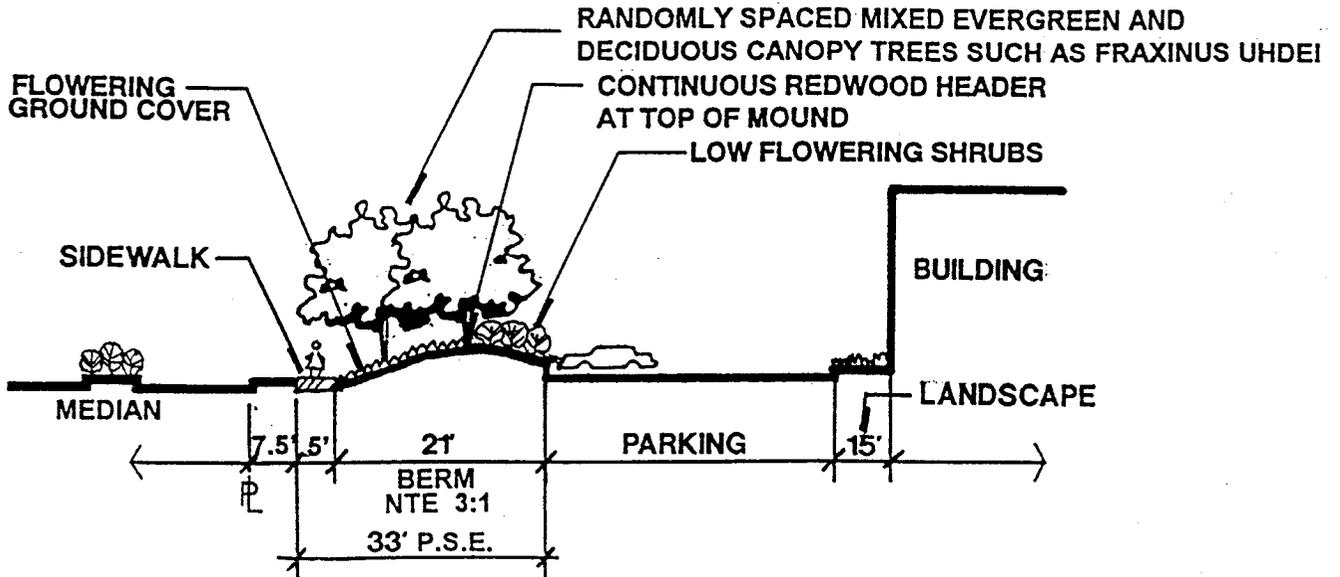
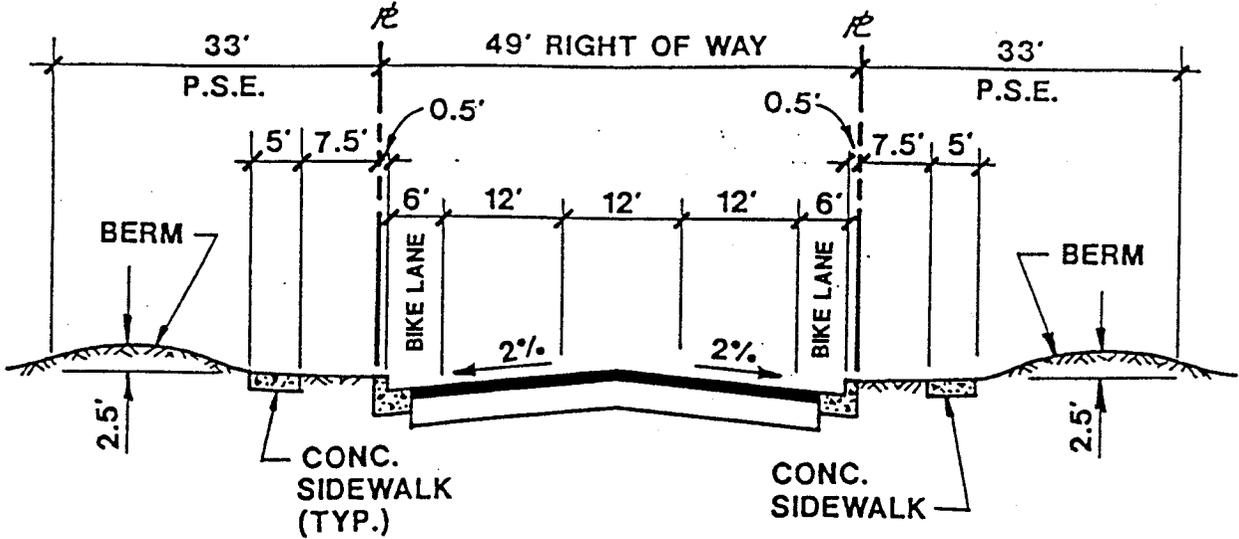
Exhibit 2-E

Business Park/Light Manufacturing Development

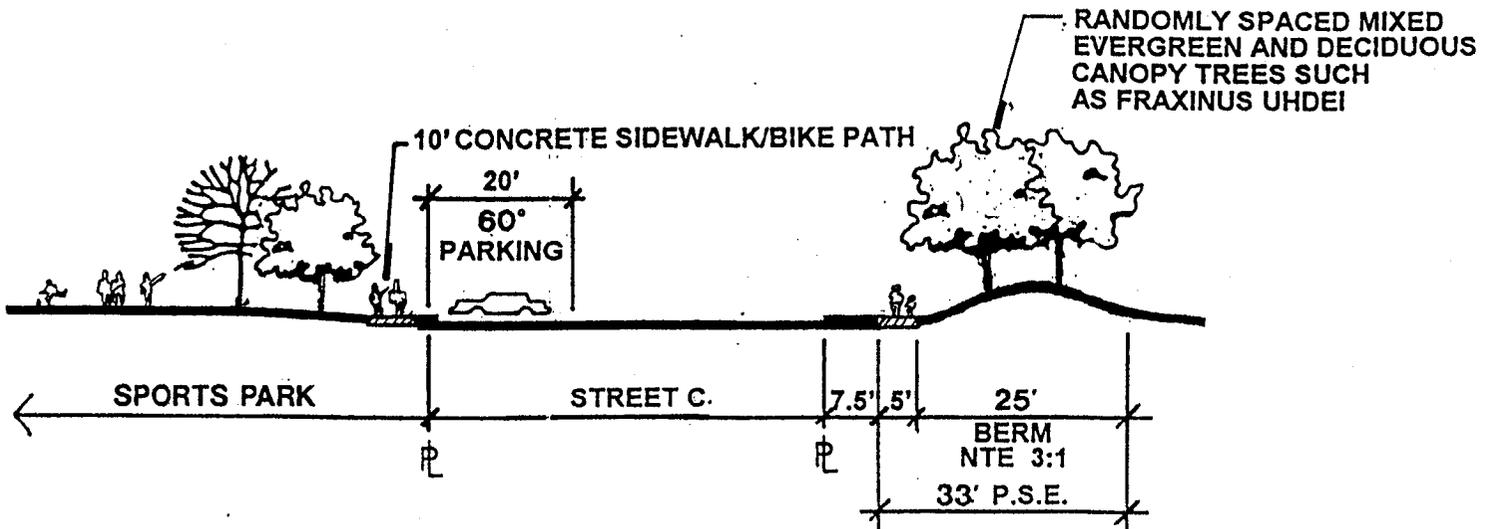
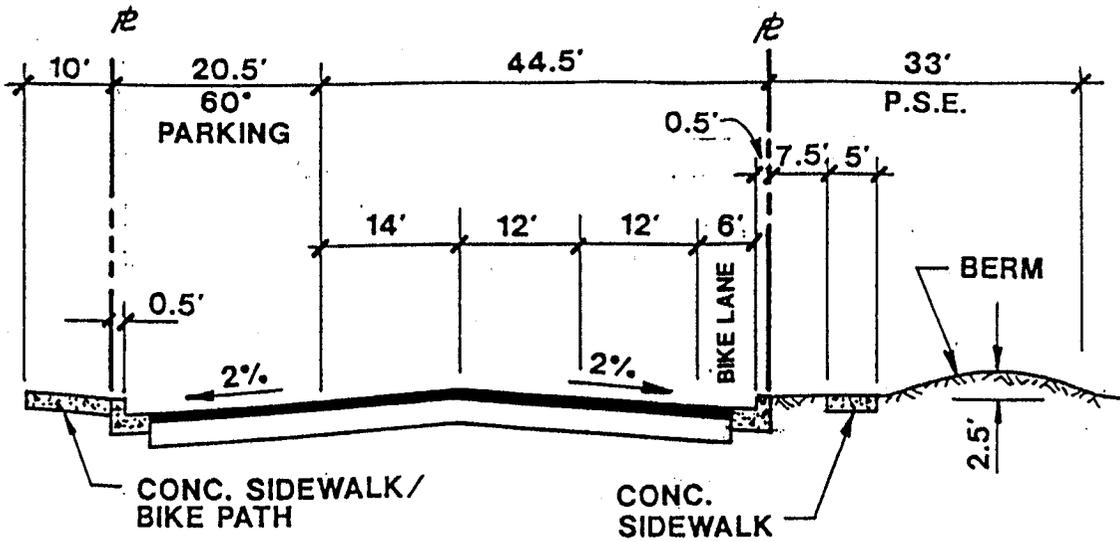
① STREET A EAST



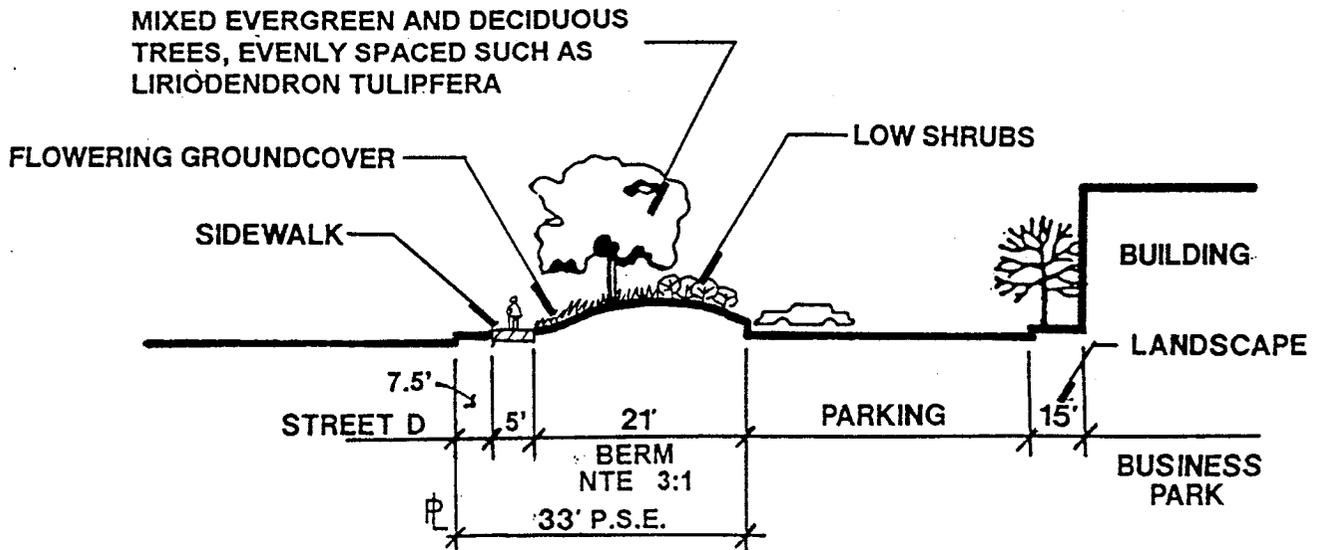
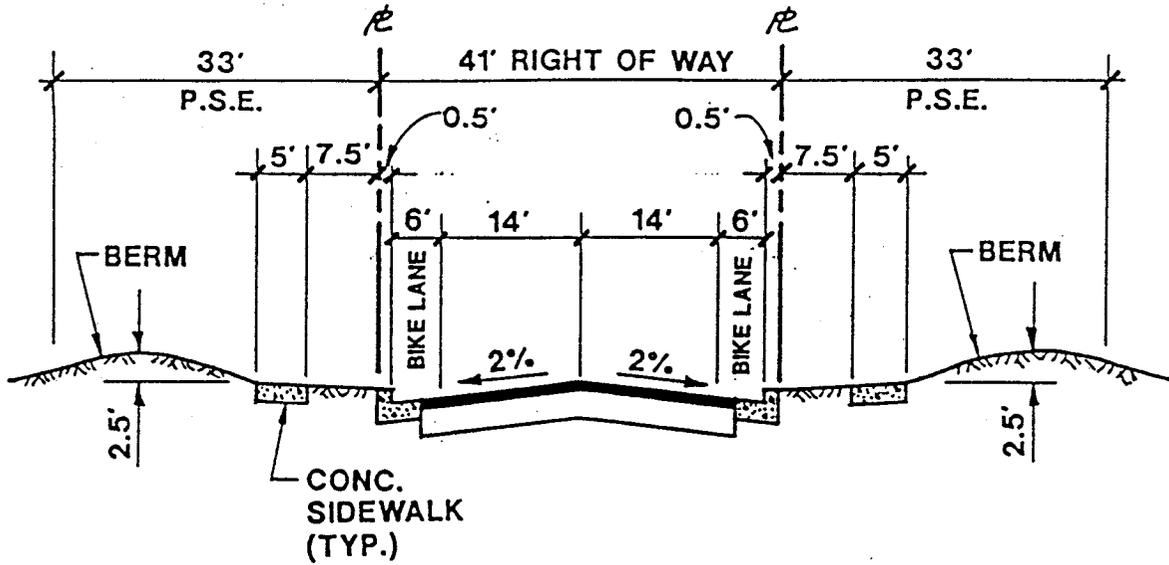
2 STREET B



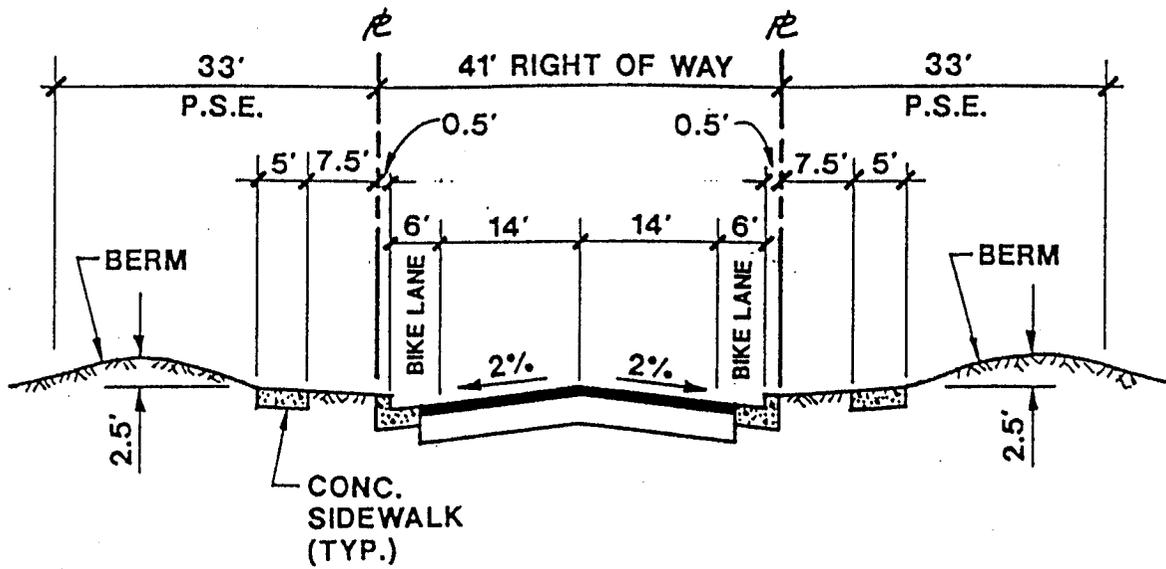
3 STREET C



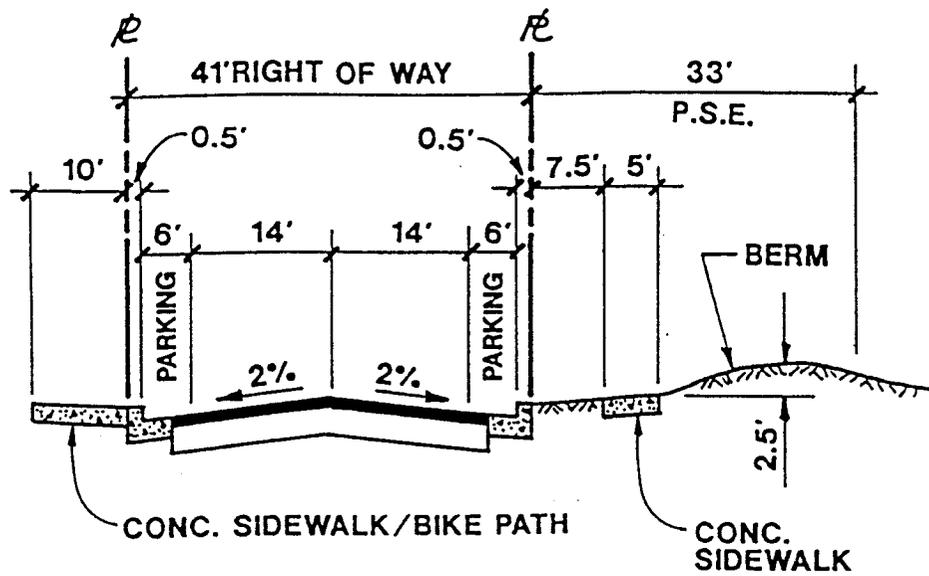
④D STREET D (CUL-DE-SAC)



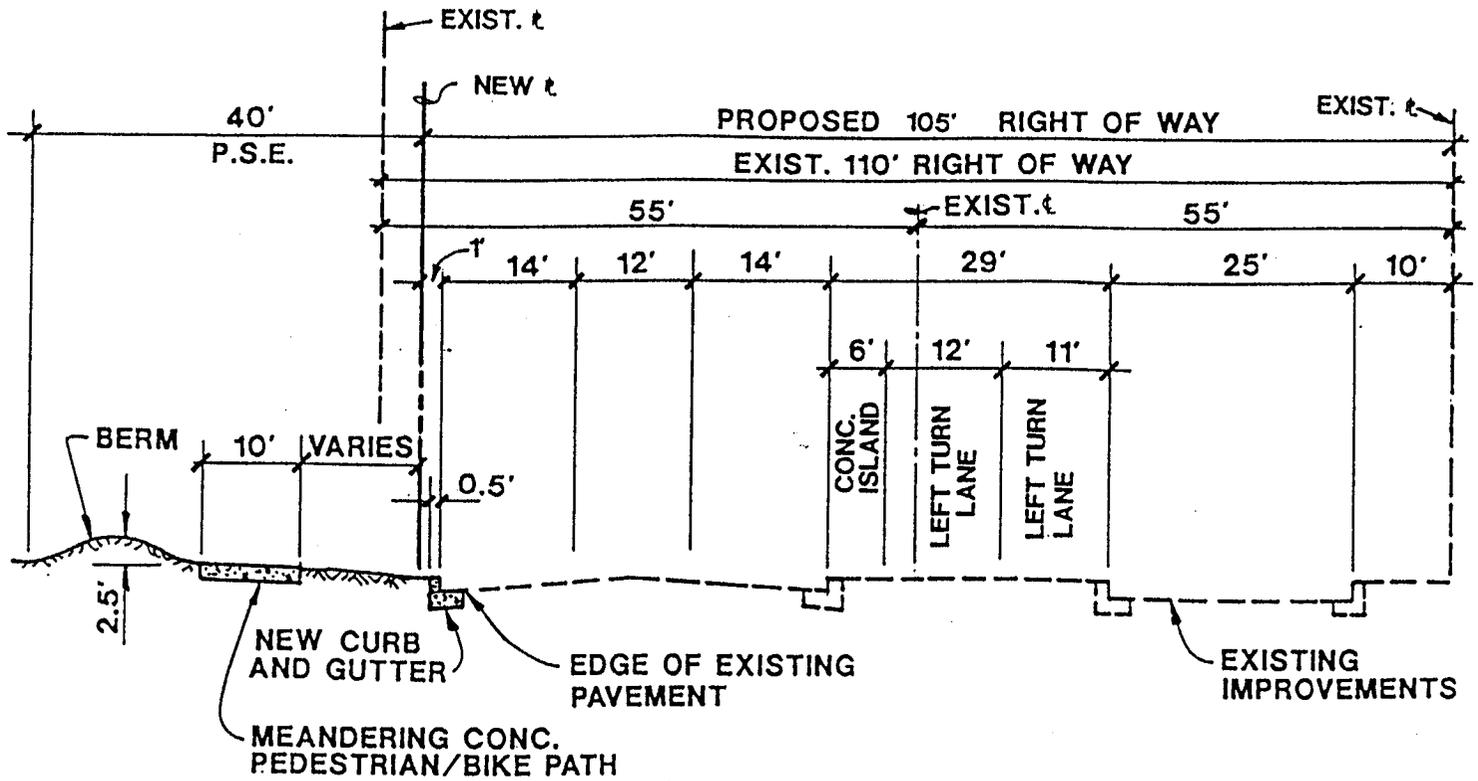
4F STREET F



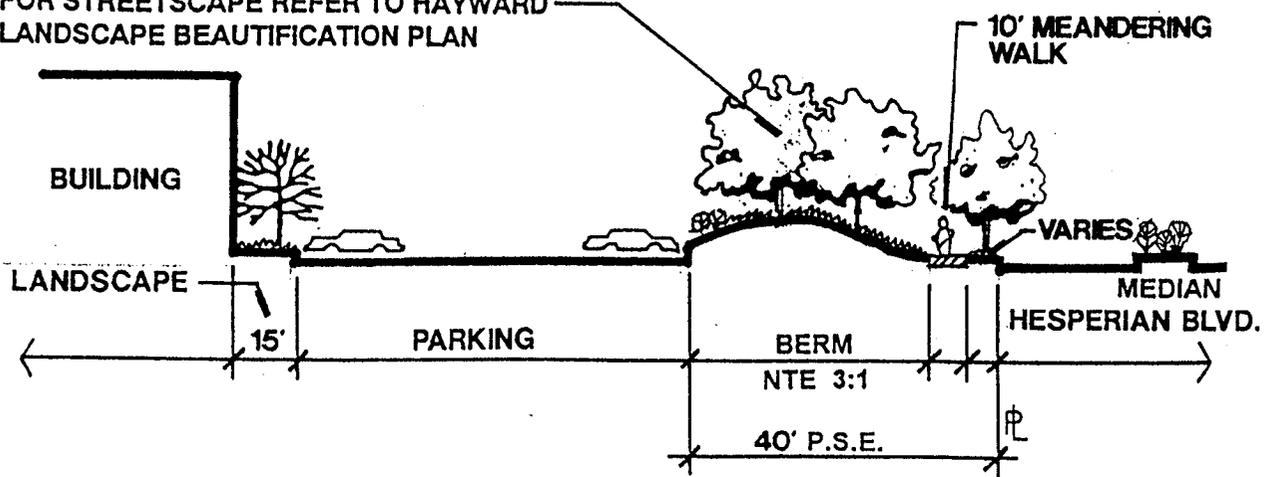
5 STREET E



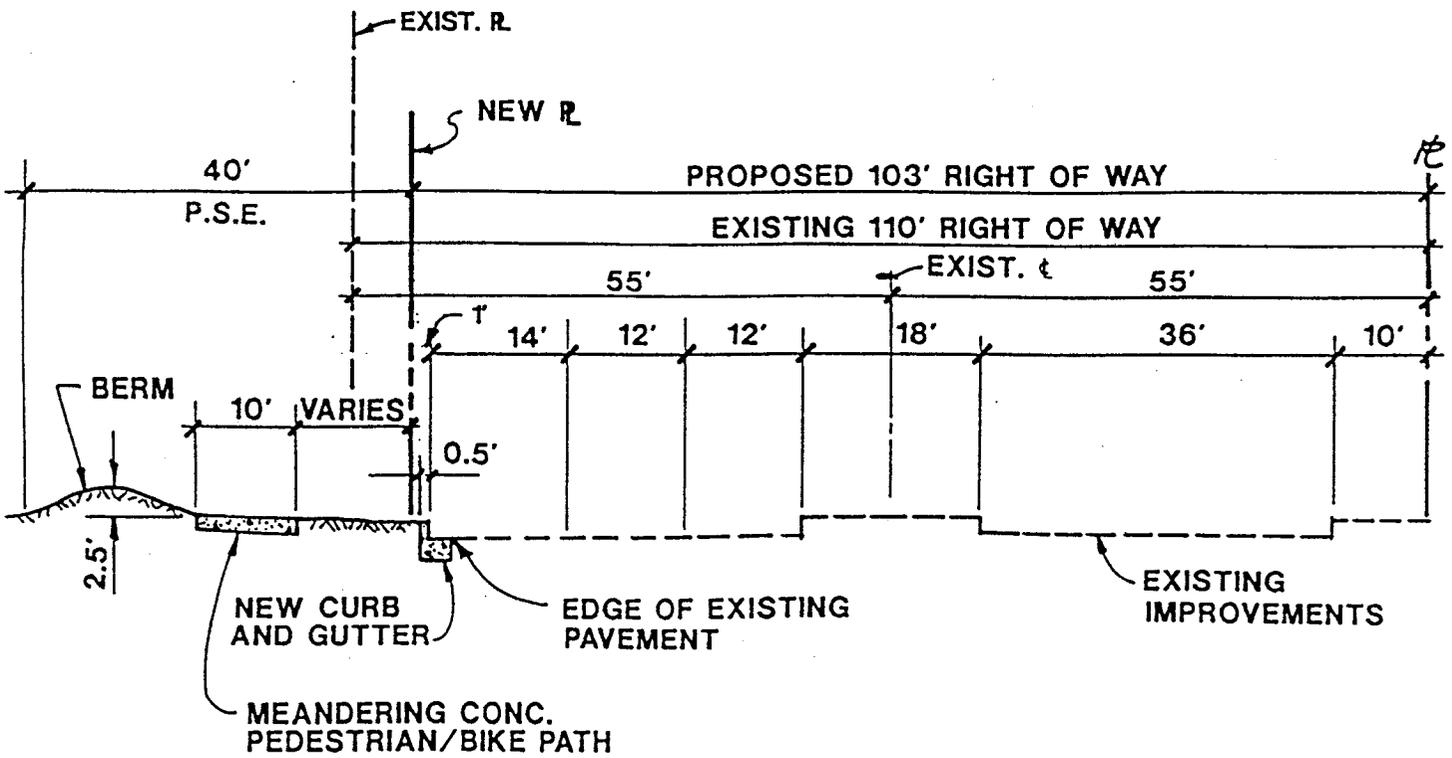
⑨ HESPERIAN BLVD. BETWEEN INDUSTRIAL BLVD. AND TRIPALDI WAY



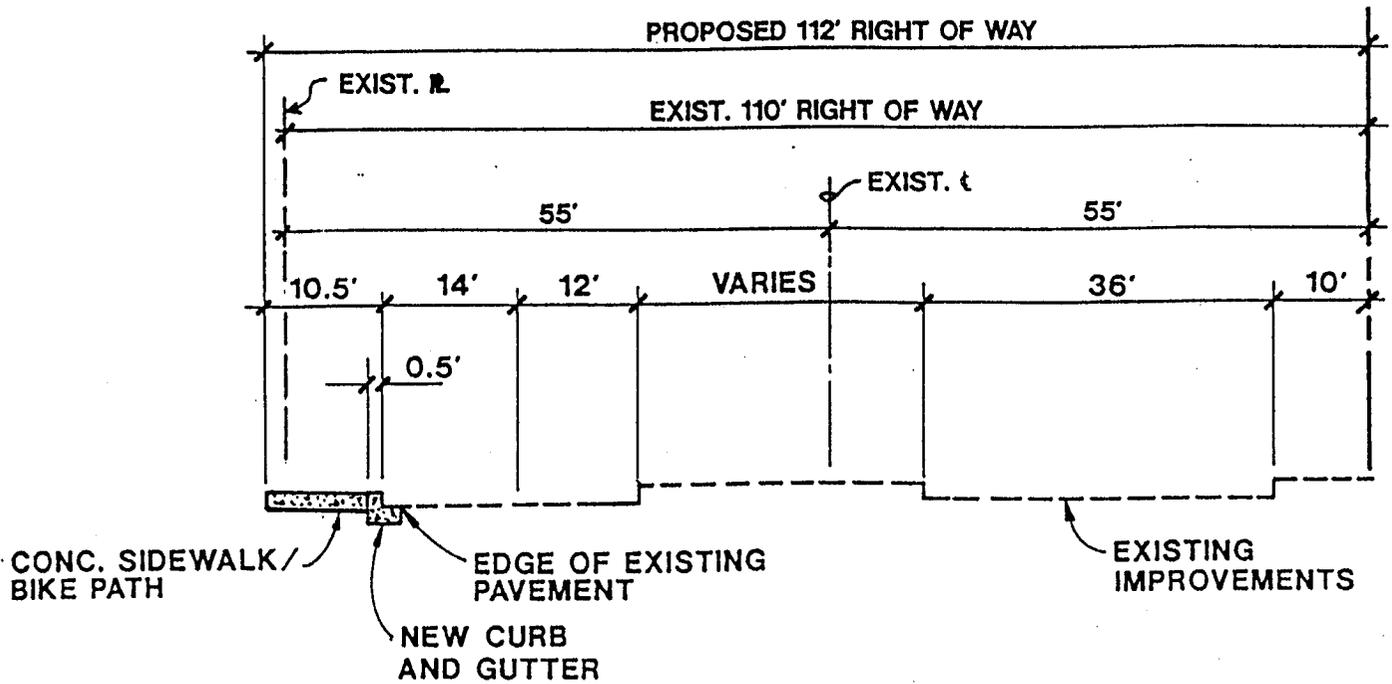
FOR STREETSCAPE REFER TO HAYWARD LANDSCAPE BEAUTIFICATION PLAN



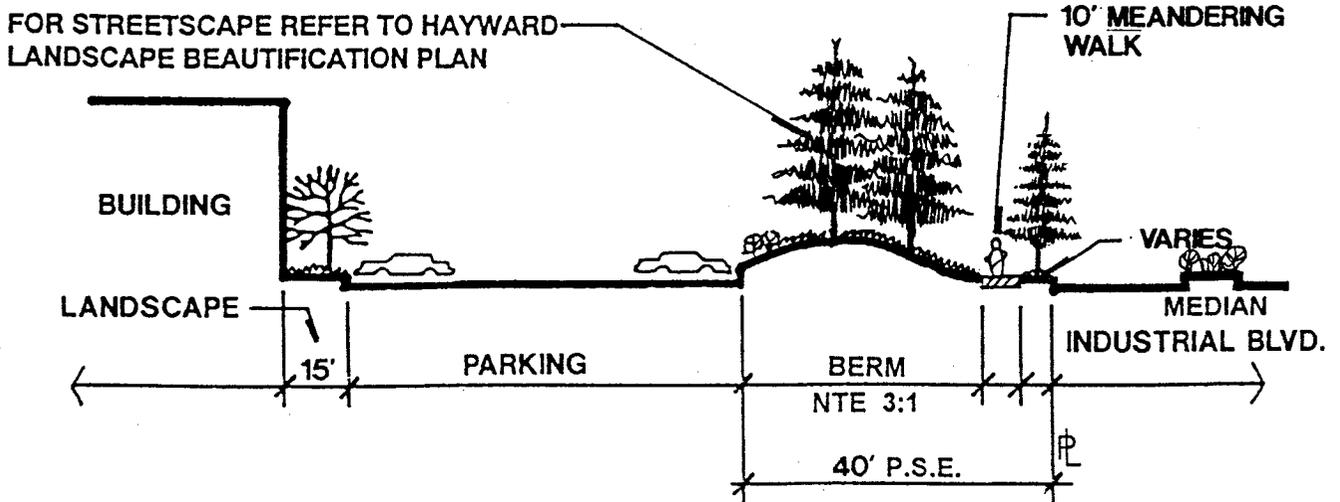
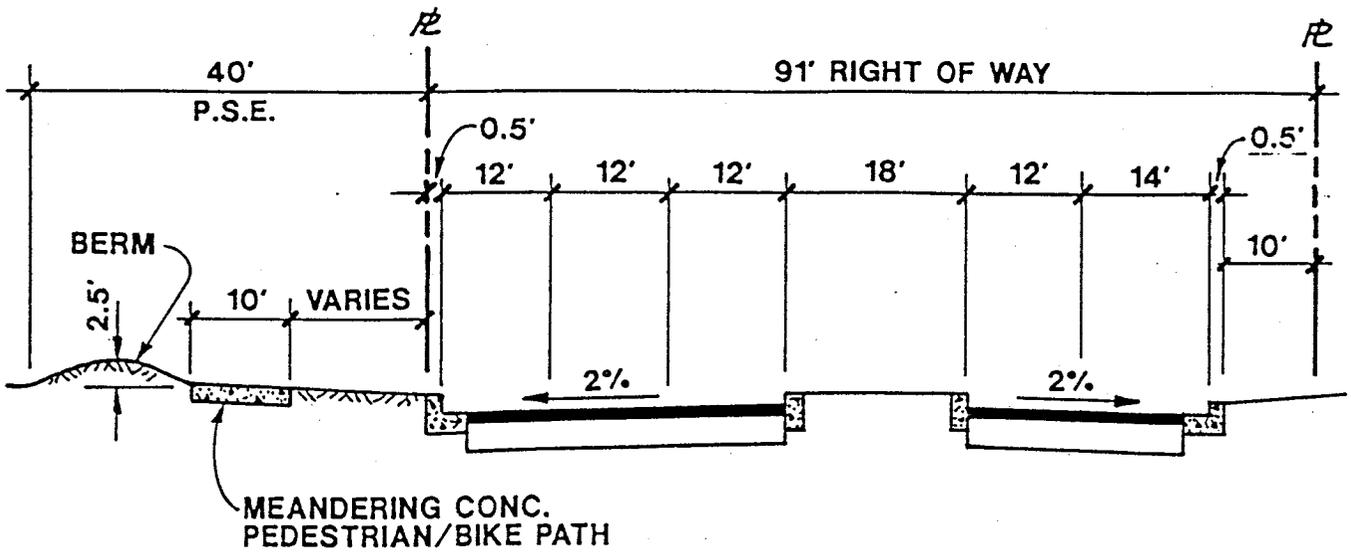
⑩ HESPERIAN BLVD. – TRIPALDI WAY
TO NORTH PEPSI DWY. (STREET C)



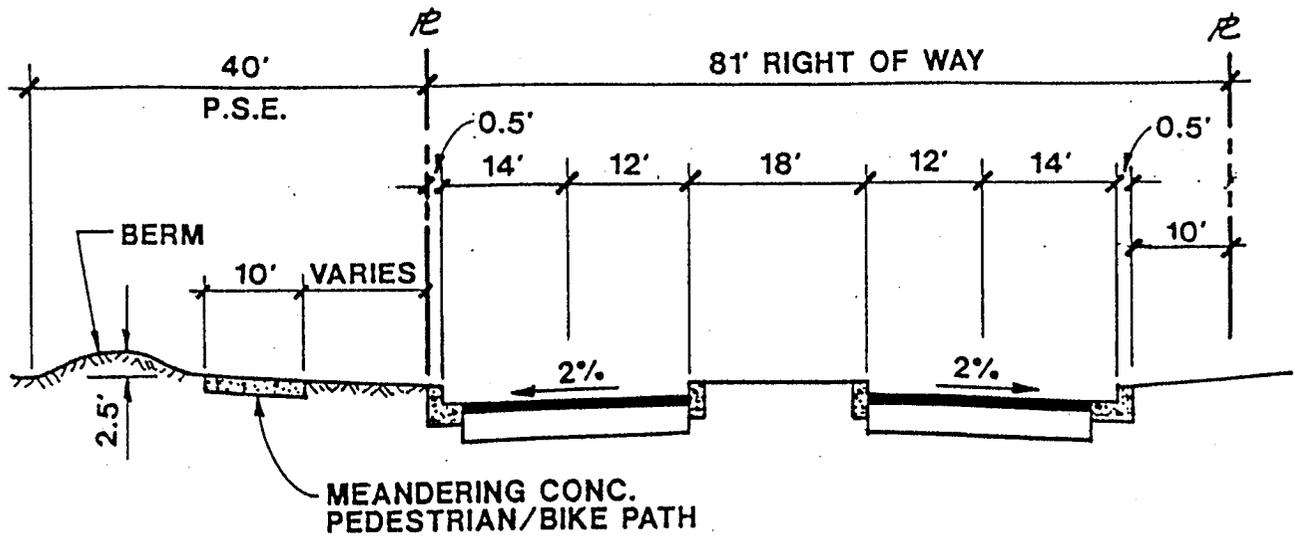
**11 HESPERIAN BLVD. – NORTH PEPSI DREVEWAY (STREET C)
TO SOUTH PEPSI DRIVEWAY**



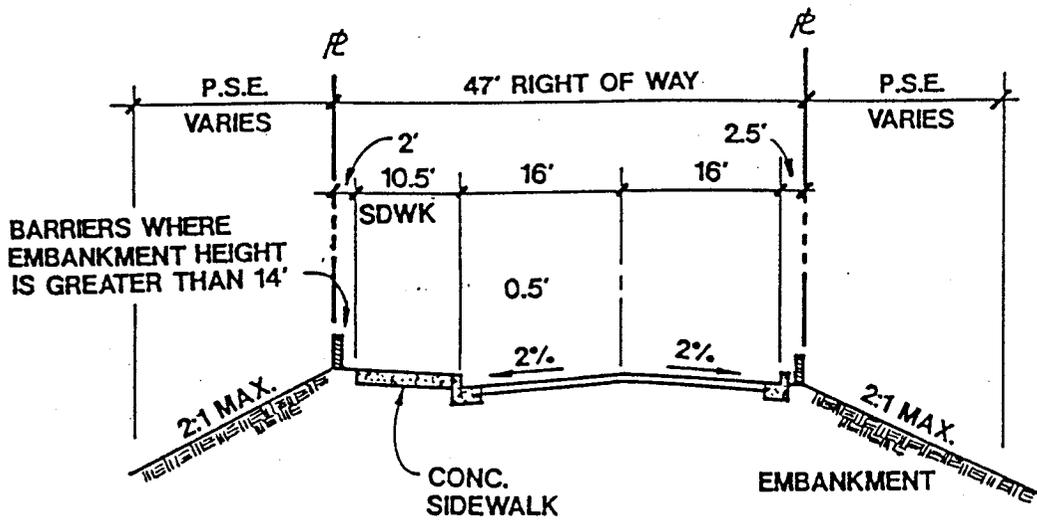
12 INDUSTRIAL BLVD. – EAST OF STREET B



⑬ INDUSTRIAL BLVD. – WEST OF STREET B

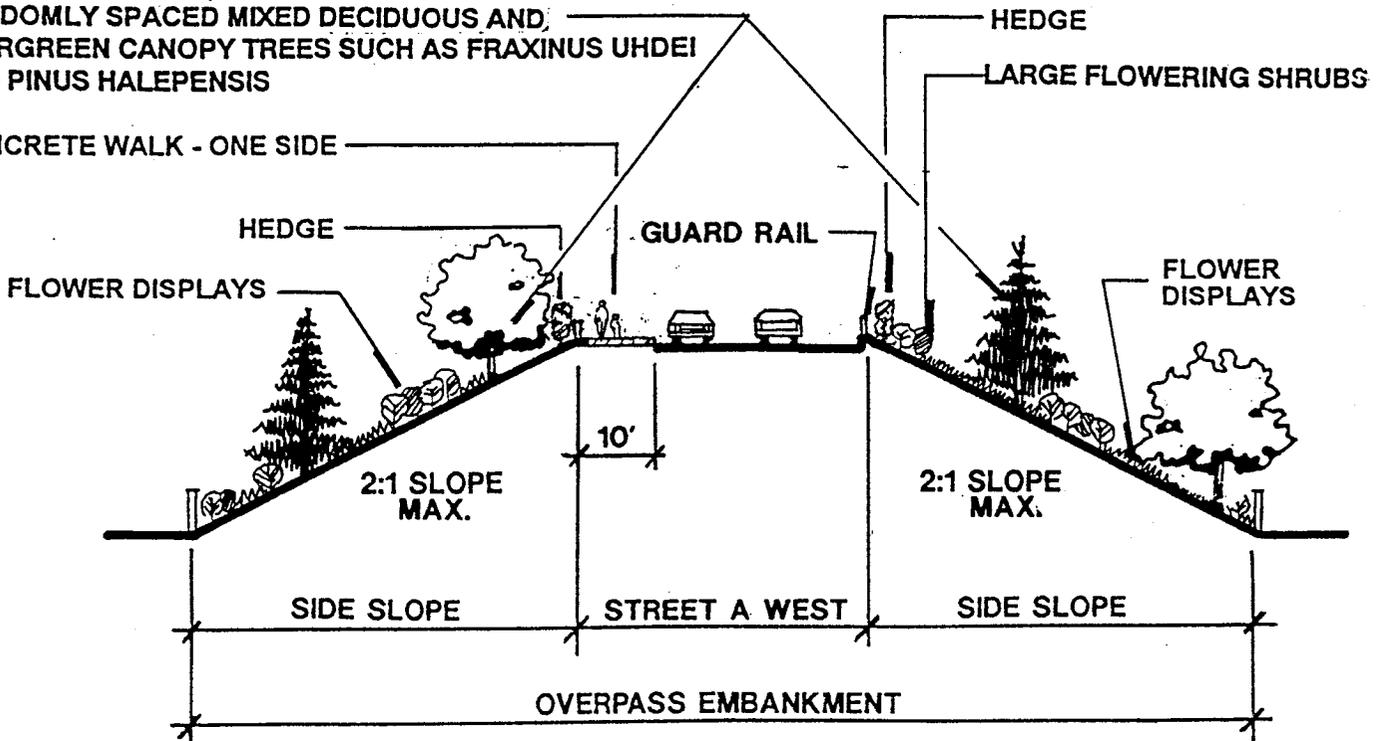


14 STREET A WEST

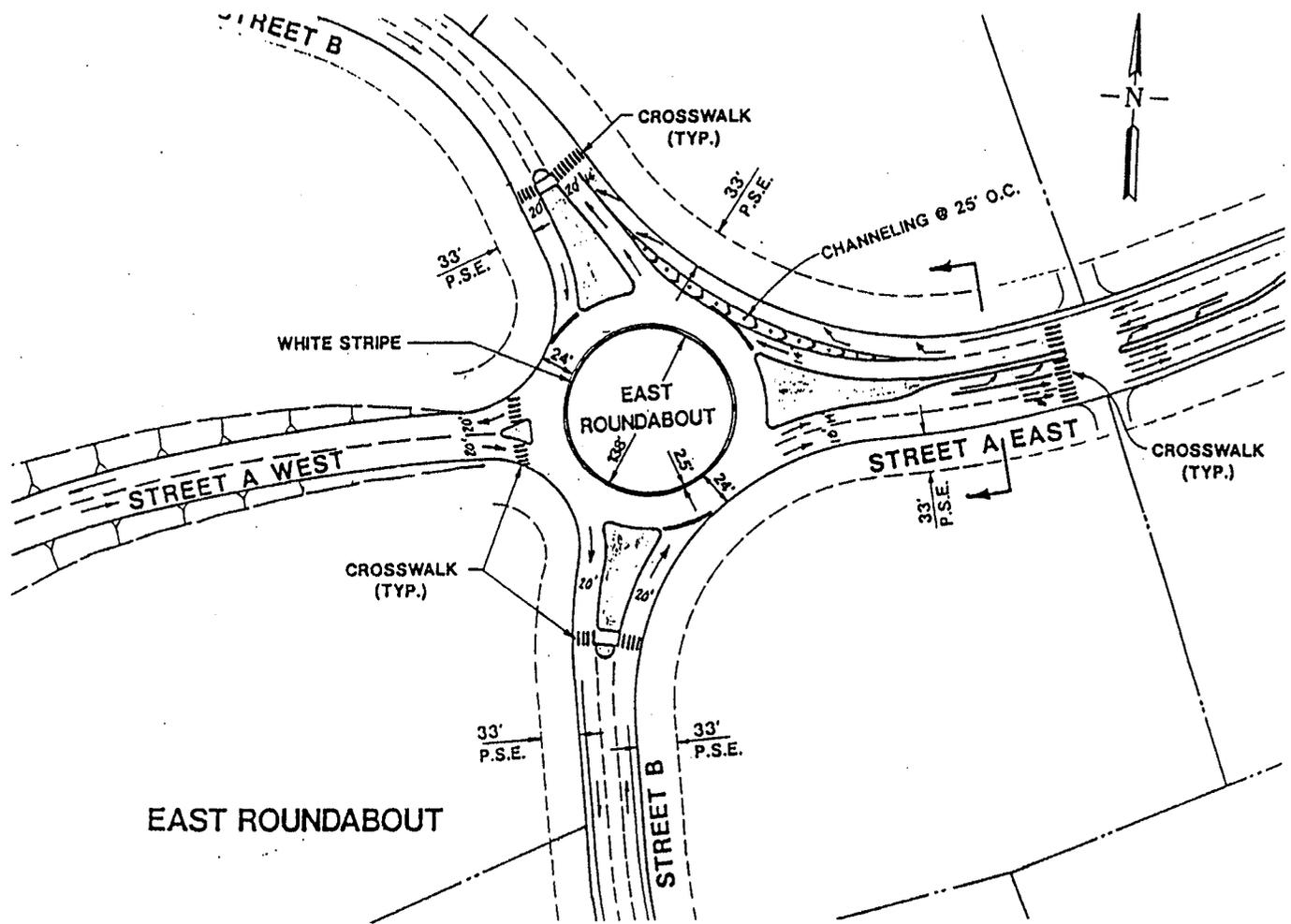


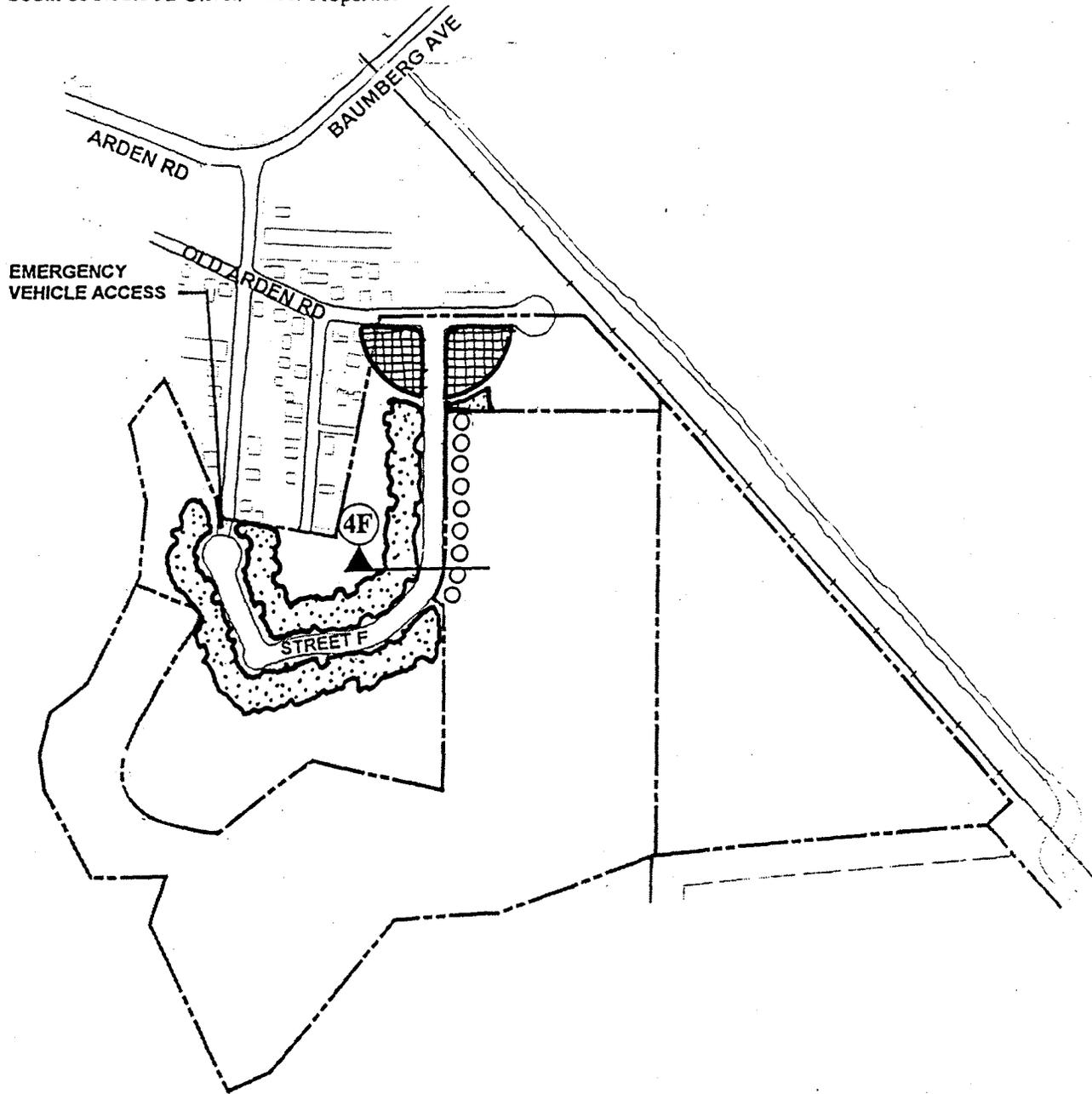
RANDOMLY SPACED MIXED DECIDUOUS AND EVERGREEN CANOPY TREES SUCH AS FRAXINUS UHDEI AND PINUS HALEPENSIS

CONCRETE WALK - ONE SIDE



15 EAST ROUNDABOUT
PLAN VIEW





LEGEND

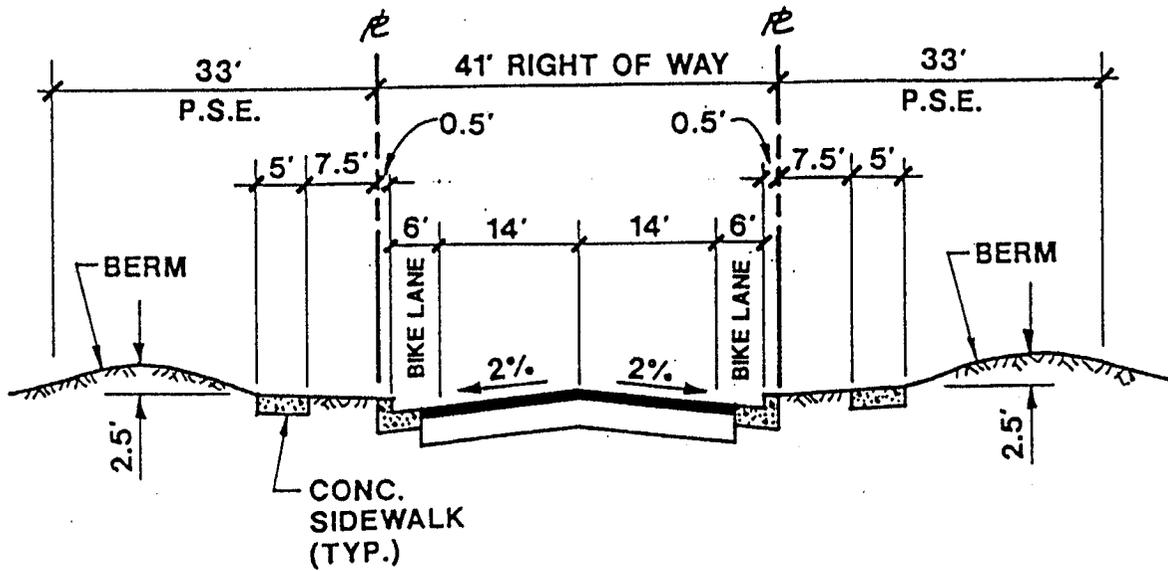
(4F) LIGHT MANUFACTURING CUL-DE-SAC
40' R.O.W.



**WEBER LIGHT MANUFACTURING PUBLIC CORRIDOR/PSE
CROSS-SECTION KEY**

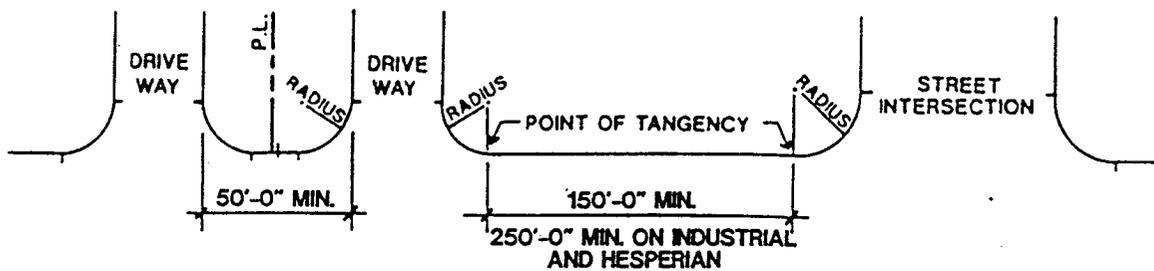
Exhibit 2-F

④F STREET F

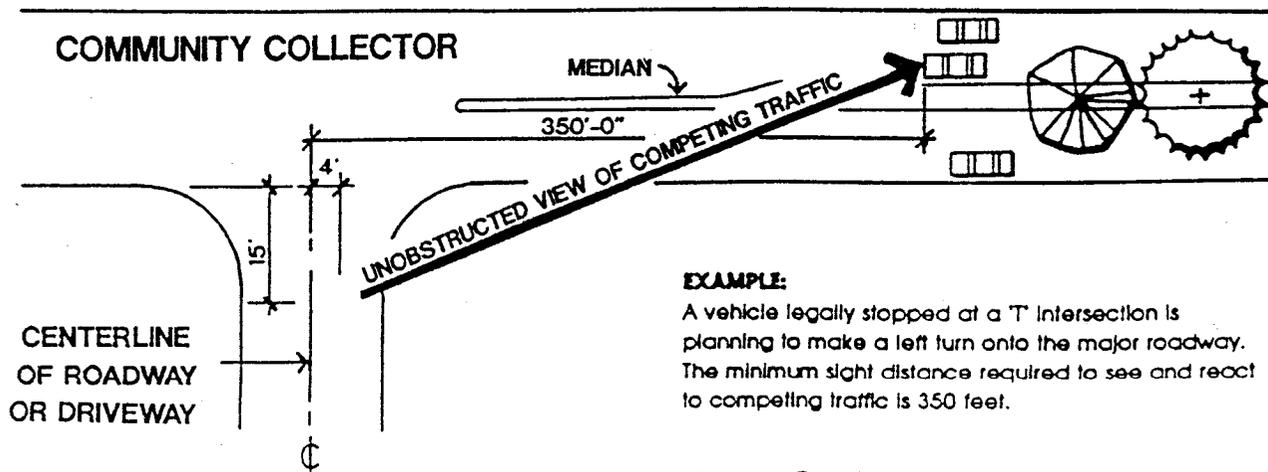


2.5.2 DRIVEWAYS

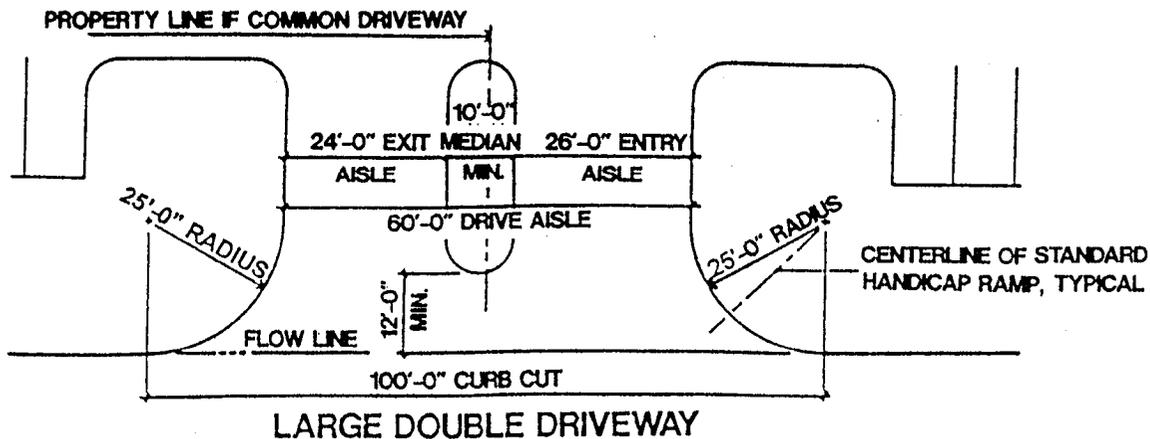
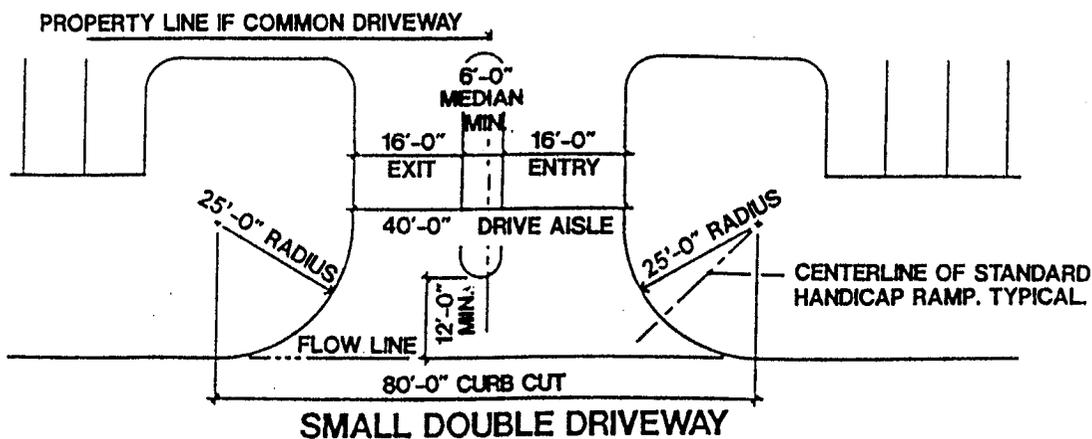
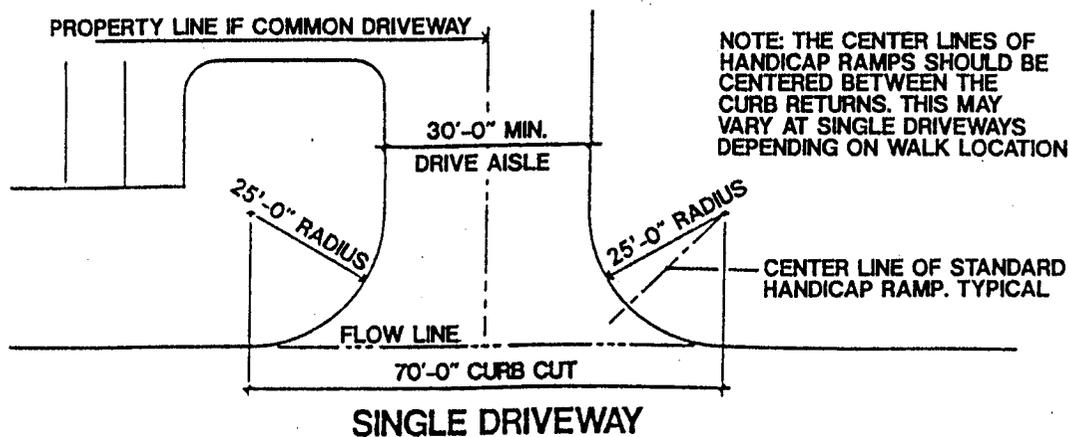
A. Driveway curb cuts shall not be constructed within 250' of street intersections from Street A or on Street B from its intersection with Street A or its intersection with Industrial Blvd, or within 150' of all other street intersections. No more than two curb cuts along a single property frontage shall be allowed unless specifically approved by the City. Such approval shall not be considered unless the property owner utilizes a common driveway ingress and egress easement with an abutting property. Driveway access to Hesperian Blvd. and to Industrial Blvd. shall be limited to common driveway ingress and egress easements on all sideyard property lines and shall be allowed only on properties located east of the Street B intersection with Industrial Blvd. and north of the Street C intersection with Hesperian Blvd. There shall be a 50' minimum between the drive aisles of adjacent parcels on all other streets.



- B. Safe sight distances shall be maintained for intersections and driveway egresses. The following criteria shall be used when designing roadway landscaping to prevent a motorist's vision from being obscured by signs, trees or shrubbery. Confirmation of adequate sight distance is the responsibility of the parcel developer.
1. For each 10 mph of design vehicle speed (posted + 10 mph), an unobstructed view of 100' in the direction of oncoming or competing traffic is required.
 2. The corner sight distance is measured from a point 4' from the centerline of the driveway or minor road, at least 15' from the edge of the major road pavement, and at an eye height of 3.5' on the minor road to an object (car) at a height of 4' on the major road.

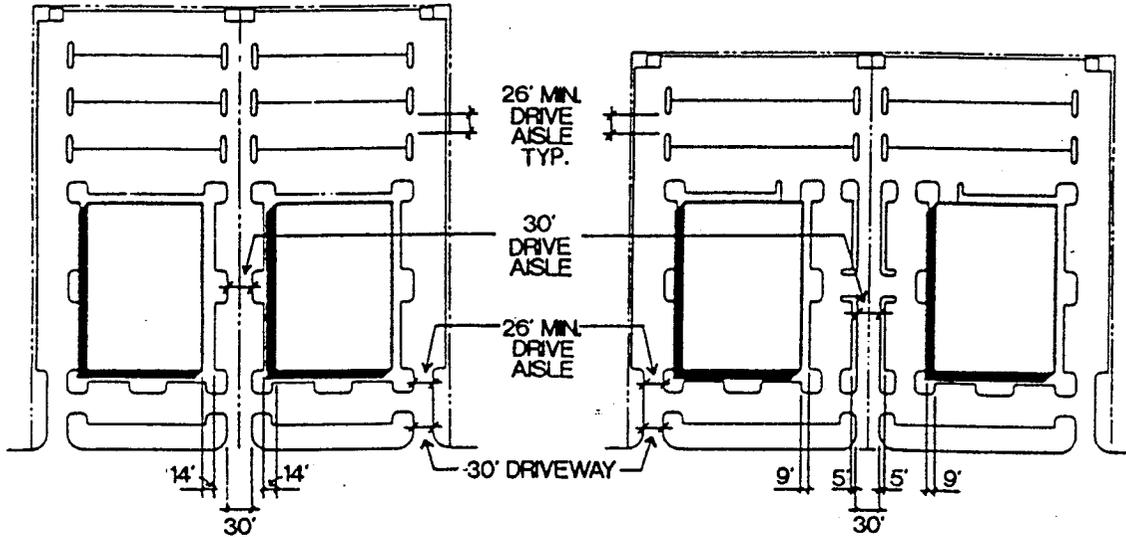


C. Vehicular access to parcels will be provided by one of three types of driveways as illustrated below. Driveway medians shall be set back a minimum of 12' from the street curb. Regardless of the type of driveway used, the minimum on-site drive aisle width shall be 26'. Where adjacent sites have a common driveway access, any driveway type may be used and the minimum on-site common aisle width shall be 30', as described in Section 2.5.D.



D. Common Driveway for Adjacent Parcels

Illustrated below are the sideyard setback conditions where adjacent sites have a common drive aisle. The minimum on-site drive aisle width shall be 26', except at common driveway site access where it shall be 30'. Non-common drive aisles may taper from 30' at driveway to 26' at drive aisles. The taper shall not begin before the point of tangency of the driveway radius.



30' DRIVEWAY

**COMMON DRIVEWAY ACCESS WITH
ADJACENT PARKING SIDEYARD SETBACK**

30' DRIVEWAY

**COMMON DRIVEWAY ACCESS WITH
PARKING SIDEYARD SETBACK**

* Each of the 14' sideyard setbacks include 5' transferred from the property line where adjacent parking occurs, as well as similar landscape treatment on both sides.

2.5.3 PROJECT MONUMENTATION

As indicated on the Landscape Master Plan there are two types of gateways within the South of Route 92 Plan Area. Each of these gateways will receive treatment appropriate to its function and importance.

A. Main Project Gateway

Located on Hesperian Blvd. across from the Pepsi plant this entry occupies both westerly quadrants of the Street A intersection. Large masonry elements backed by semi-circular plantings of tall vertical trees will announce the arrival and entry place from a distance.

The entry treatment may include textured pavement at the corners and shall include colorful ground cover plantings. In addition to the planting at this and other gateways, special vertical lighting is recommended to establish a unique character for the new community.

B. Secondary Project Gateways

Three other entry points serve the community. An entry with full turning movements located on Industrial Blvd. accesses the Business Park at Street B. A second entry on Hesperian Blvd. at Street C accesses the lower Business Park and the Sports Park. This entry will be limited to right-in right-out movements only. The third entry from the extension of Baumberg/ Arden Roads will serve the Light Manufacturing west of the railroad tracks. Each of these entries will exhibit consistent monumentation, vertical tree plantings and other improvements similar to the main project gateway with a corresponding reduction in scale commensurate with their importance.

C. Gateway Landscaping

Each gateway would receive treatment appropriate to its function and importance. The following discussion highlights the proposed treatment of these elements.

1. The Main Project Gateway

The easterly intersection of Street A and Hesperian Boulevard should dramatically identify the entrance to the community. Double rows of vertical evergreen trees planted to accentuate the gateway's two corners will announce the entry place from a distance. As one approaches the intersection, the landscape treatment should consist of textured pavement at the corners and raised planters as a background for signage identifying the name of the community. In addition to the planting, in this and other gateways, special vertical lighting is recommended to establish a unique character.

2. Secondary Light Manufacturing Park Gateways

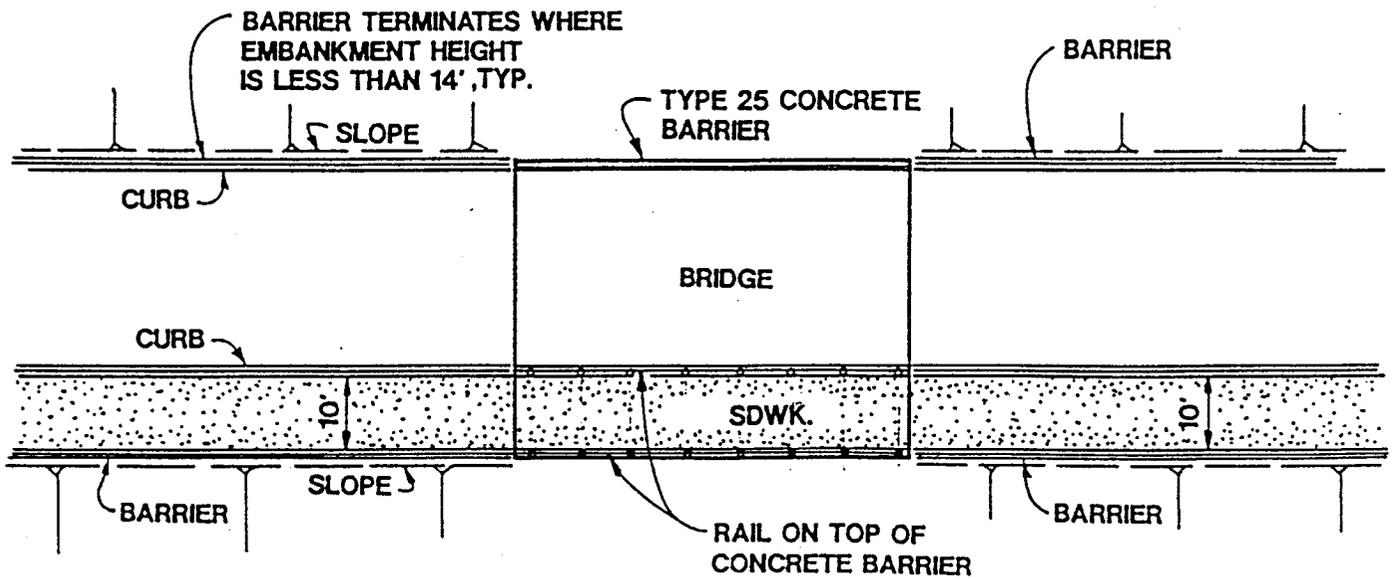
Three locations exist for these elements; at Industrial Boulevard westerly of Hesperian Boulevard, on the westerly side of Hesperian Boulevard south of the main project entry and at Arden Road. At each of these gateways planting and other improvements reflective of those used at the main project gateway will be utilized. Development intensity will be reduced to reflect the secondary nature of these elements.

2.5.4 OVERPASS AT RAILROAD TRACKS

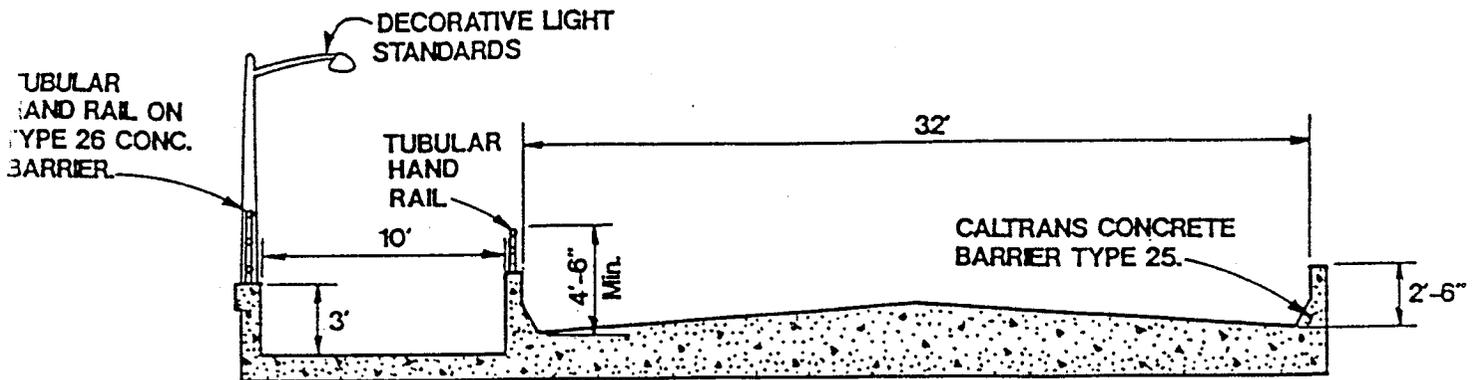
- A. Approach ramp side slopes must be of soil of no greater than 2:1 gradient, and landscaped in such a manner as to soften the visual impact of the structure.
- B. A 10' wide concrete walk adjacent to the curb shall be constructed on one side of the overpass roadway to accommodate pedestrians and bicycles.
- C. The portion of the overpass roadway directly over the railroad right-of-way shall be protected by a 6' high decorative iron fence, and shall include distinctive light standards. The exterior

curb of the street and the exterior of the sidewalk shall be protected by a 2 1/2' K rail from both sides of the overpass down to finished grade.

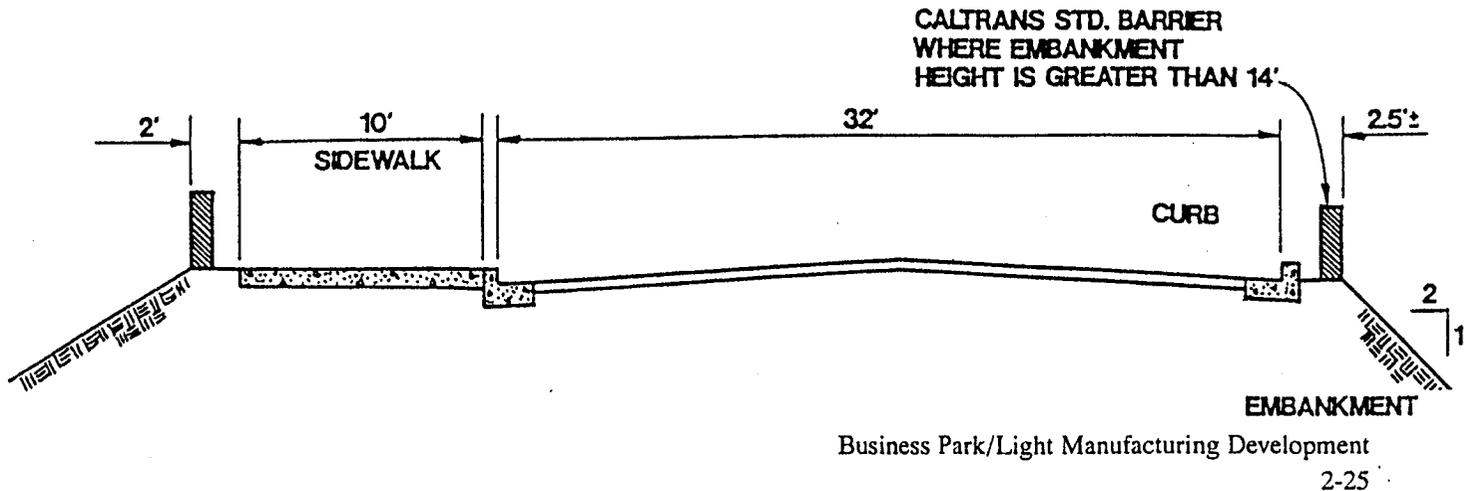
PLAN VIEW



SECTION AT BRIDGE



SECTION AT EMBANKMENT



2.6 PUBLIC SERVICE EASEMENT (PSE)

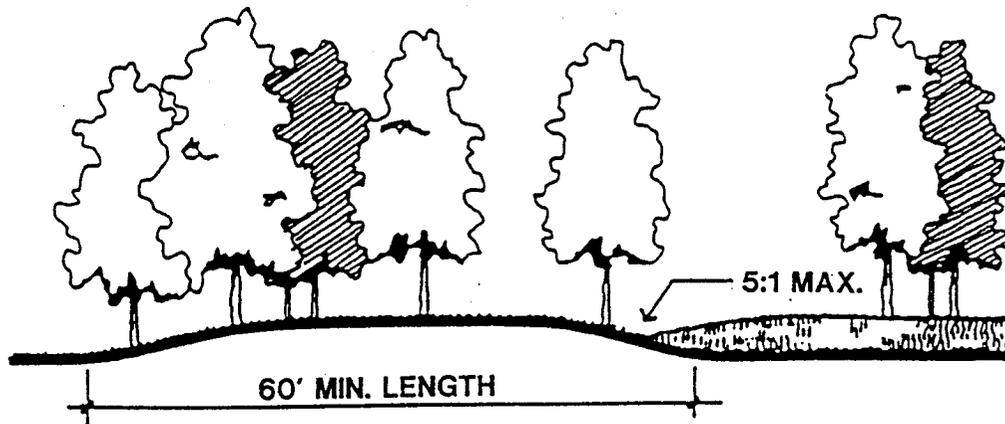
2.6.1 PSE PLANTING

- A. Trees: the quantity of trees planted in the PSE shall be one 24" box size tree per 650 square feet. For Commercial/Retail sites the quantity may be reduced to one tree per 1,000 square feet. Tree size requirements are shown in Section 2.7.5. Where an industrial or commercial use is adjacent to a residential use, a minimum of 15-gallon trees shall be planted every 20 linear feet.
- B. All trees in the PSE shall be planted a minimum of 3' (unless otherwise indicated) from structures and other improvements (curbs, sidewalks, walls & fences, underground trenches, etc.). Tree limbs extending into the street shall have a minimum ground clearance of 12'. Tree trunks shall be kept a minimum of 15' from all project street lights. A minimum 30" high continuous shrub screen, berm or wall is required to buffer parking areas from the street.
- C. The Lawn Seed Variety for all PSEs shall be either Tall Fescue (*Festuca Eliator*) or Dwarf Tall Fescue.

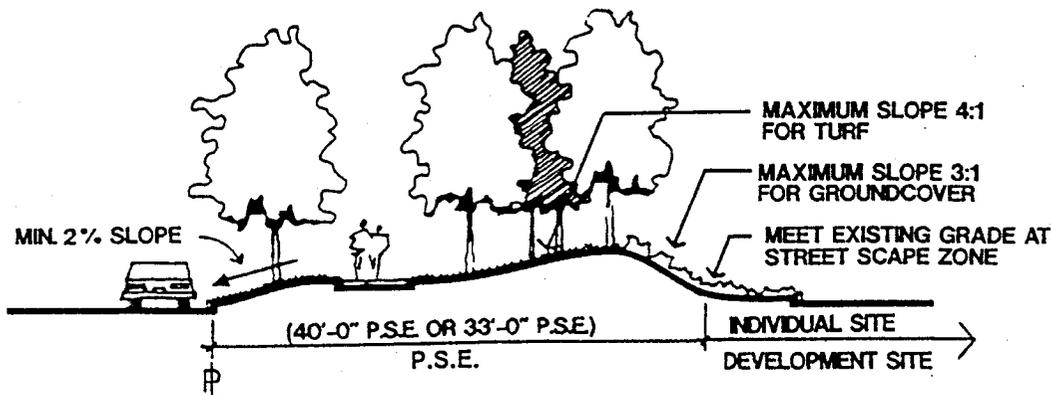
Landscape Grading and Drainage

1. All planting areas shall be graded no less than 2% to insure adequate drainage.

2. All landscape elements shall be designed and coordinated with adjacent parcels so that there is a smooth transition from one parcel to the next.
3. All landscape areas in the PSE shall include long, graceful earth berms that utilize the whole parkway width to achieve a minimum height typically 3' above adjacent street curb and typically varying from 3'-6" to 5' above parking surface.
4. For Commercial/Retail sites, PSE berming heights may be reduced to a minimum of 2' above adjacent street curbs.
5. In addition to providing consistent form, the intent of this grading guideline is to screen parking. Length of berms shall be maximized where practical.



6. Typical Berm Grading: Transition from the PSE to the Development Site landscape areas shall be smooth with grades not exceeding 3:1 for groundcover and 4:1 for turf utilizing continuous planting across the easement lines.



D. PSE Irrigation

1. Above grade backflow prevention devices for domestic water shall be screened from view from the street by locating them in shrub areas or on the back side of mounds. On streets with parkways, they shall not be permitted in the area between the sidewalk and the curb. All backflow prevention devices installed within the development shall be painted forest green.
2. Double detector check valves are required in the PSE for fire line service to buildings. Above grade check valves shall be screened from view in the same manner as described above. There shall be 12" minimum clearance above finish grade to bottom of check valves. Flange by flange fittings are required. (Placement of check valves subject to change; check with City Public Works.)

E. PSE Maintenance

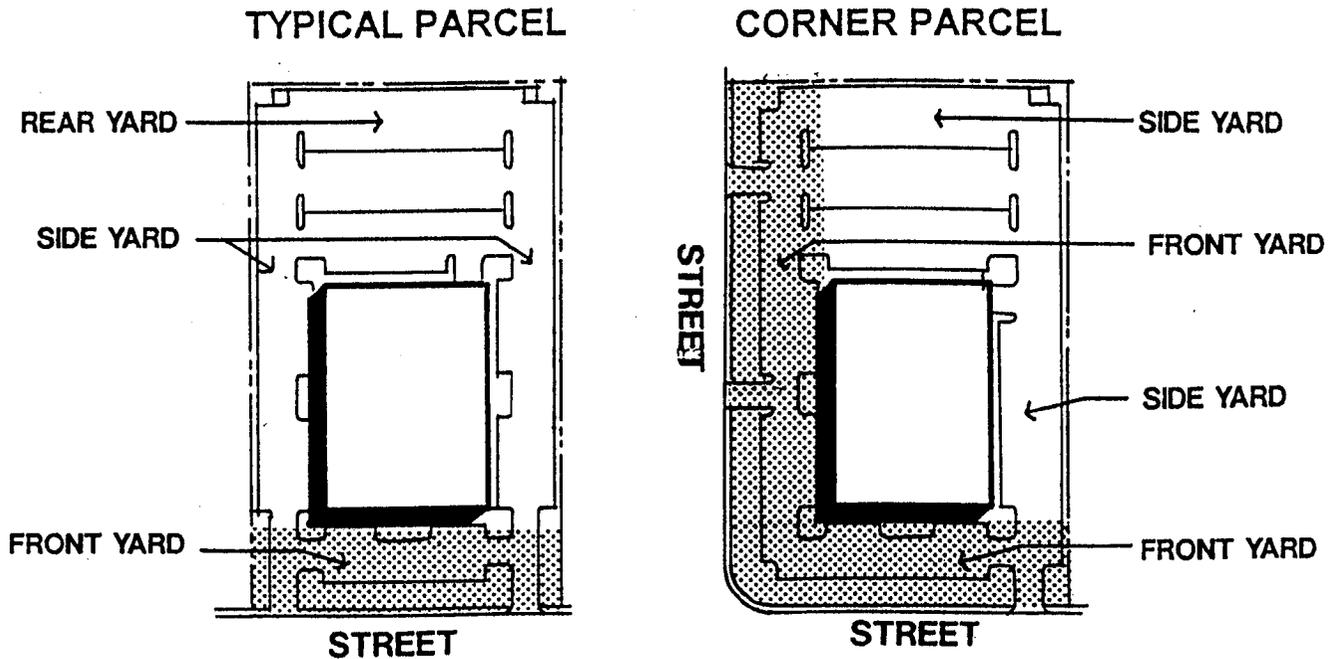
All landscape, irrigation, sidewalks, bicycle paths and utility connections within the Public Service Easement shall be maintained by the Master Developer or a commercial property owners association.

2.6.2 BUILDING PLACEMENT AND ORIENTATION

- A. The siting and orientation of a building or structure needs to reflect its functional needs, but must also be sensitive to the individual parcel's characteristics as well as to adjacent land uses and the surrounding community. It is important that the siting and three-dimensional character of each structure be considered as it relates to the specific parcel, its effect on adjacent parcels and the massing on consecutive parcels.
- B. Landscape site coverage requirements are: Business Park at 30%; Light Manufacturing at 20%, and Commercial/Retail at 25%.
- C. Each parcel shall provide a well-defined entry sequence for pedestrian and vehicular users from the street to the building. Entry onto the parcel, and connections to and into the building(s) shall be enhanced with landscaping, hardscaping and architectural design elements which work together to create a sense of arrival.
- D. A path of pedestrian travel from the street on which the building fronts to the building(s) on site shall be provided for each project.

2.6.3 FRONT, SIDE AND REAR YARD DESIGNATIONS

- A. On a typical interior parcel, the front yard will be the area adjacent to the street behind the curb; side yards are between the building and the property lines to each side; and the rear yard is the area adjacent to side yards and the rear property line.
- B. On corner parcels or parcels with streets on more than one side, the area adjacent to the streets will be considered front yards and there will be no rear yards for the purpose of establishing the required minimum landscaping depth against the building.



2.6.4 BUILDING AND PARKING AREA SETBACKS

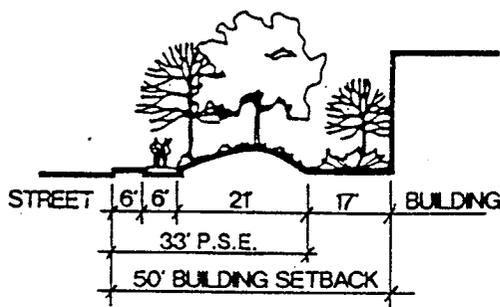
The minimum required setback dimensions from property lines to buildings and/or parking areas, and the required depths of landscaping are summarized for all front, side and rear yard conditions in Table 2-B and illustrated in the following cross-sections. These Front Yard and Side and Rear Yard setbacks apply to all buildings, parking lots, and driveways in Business Park and Light Manufacturing usage. Refer to Section 2.6.4.C for Commercial/Retail front and side yard setback. The property line is defined from the back of curb for front yards in these guidelines. Building setbacks are measured from the back of curb (property line) to the front of building structure.

**TABLE 2-B
PROPERTY LINE SETBACK CRITERIA**

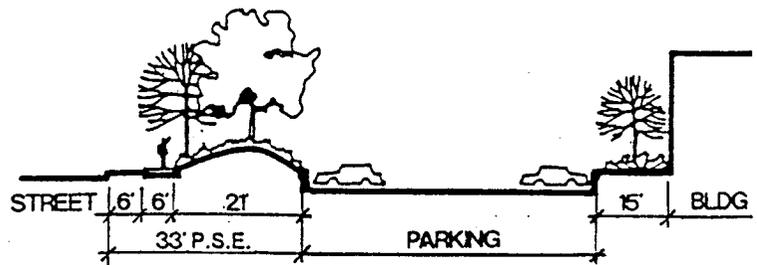
Street Frontage	Public Service Easement (PSE)	Min. Front Yard Setback	Min. Side Yard Setback	Min. Rear Yard Setback
Hesperian Blvd. - North of Street C	40'	50'	25'	25'
Hesperian Blvd. - South of Street C	33'	N/A Sports Park	N/A Sports Park	N/A Sports Park
Industrial Blvd. East of Street B	40'	50'	25'	25'
Industrial Blvd. West of Street B	33'	50'	25'	25'
Street A	40'	50'	25'	25'
Streets B, D and - North side of streets E and C.	33'	50'	25'	25'

A. Front Yard Building Setbacks

The various conditions governing front yard building setbacks are illustrated as follows:

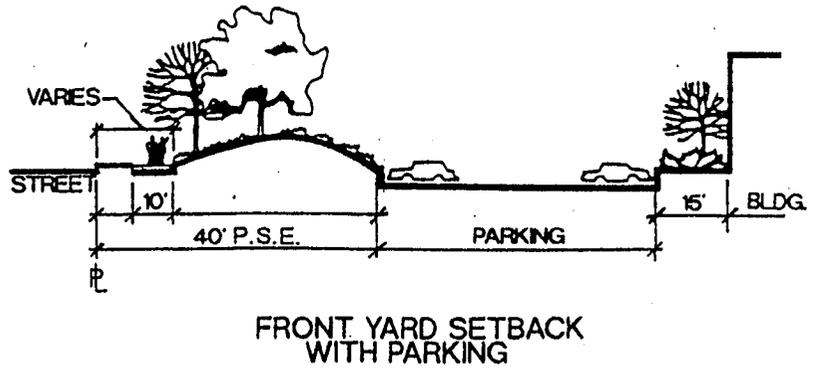
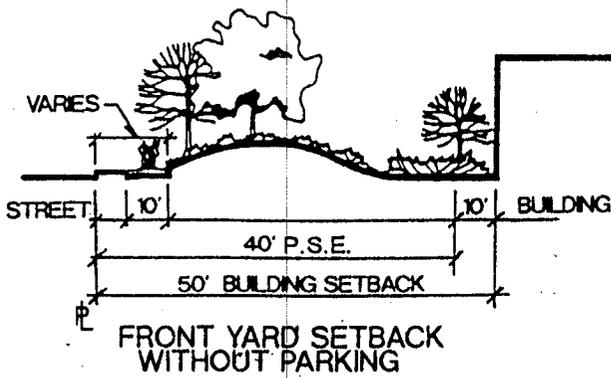


FRONT YARD SETBACK WITHOUT PARKING



FRONT YARD SETBACK WITH PARKING

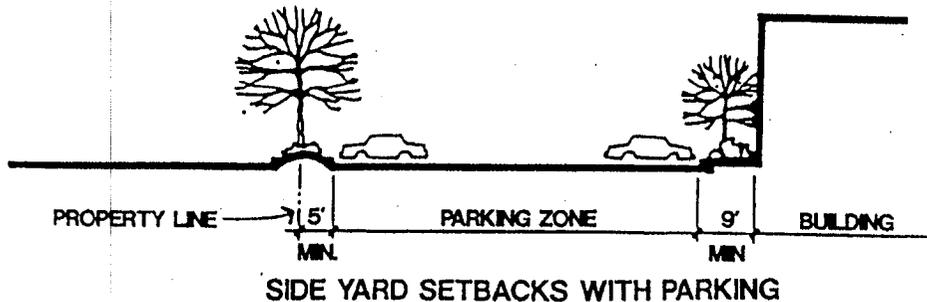
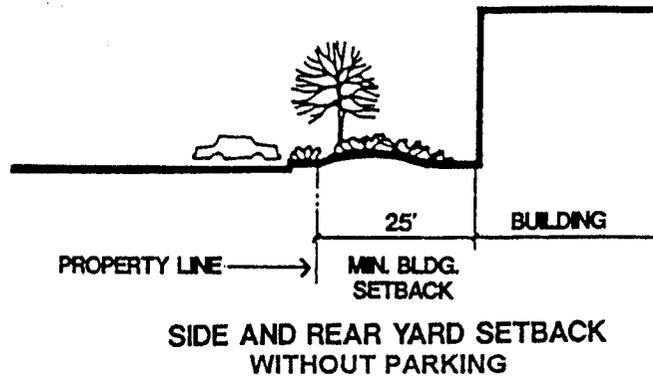
33' PUBLIC SERVICE EASEMENT

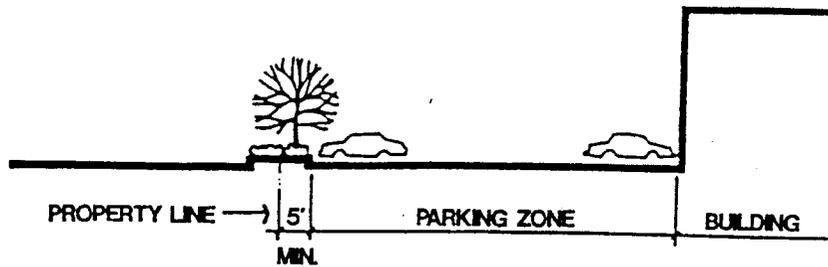


40' PUBLIC SERVICE EASEMENT

B. Side and Rear Yard Building Setbacks

When a building is located with the minimum side and/or rear yard building setback(s), all of the area between the building and the side and/or rear lot line is to be landscaped. In other conditions for side yards there shall be a minimum continuous landscaping depth of 9'-0" against the sides of the building. For rear yards no landscaping is required against the building. This requirement is in addition to the 5'-0" minimum continuous landscaping depth required at all side and rear property lines.

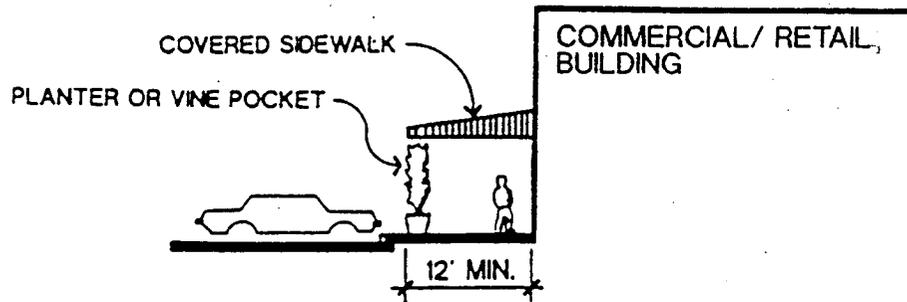




REAR YARD SETBACK WITH PARKING

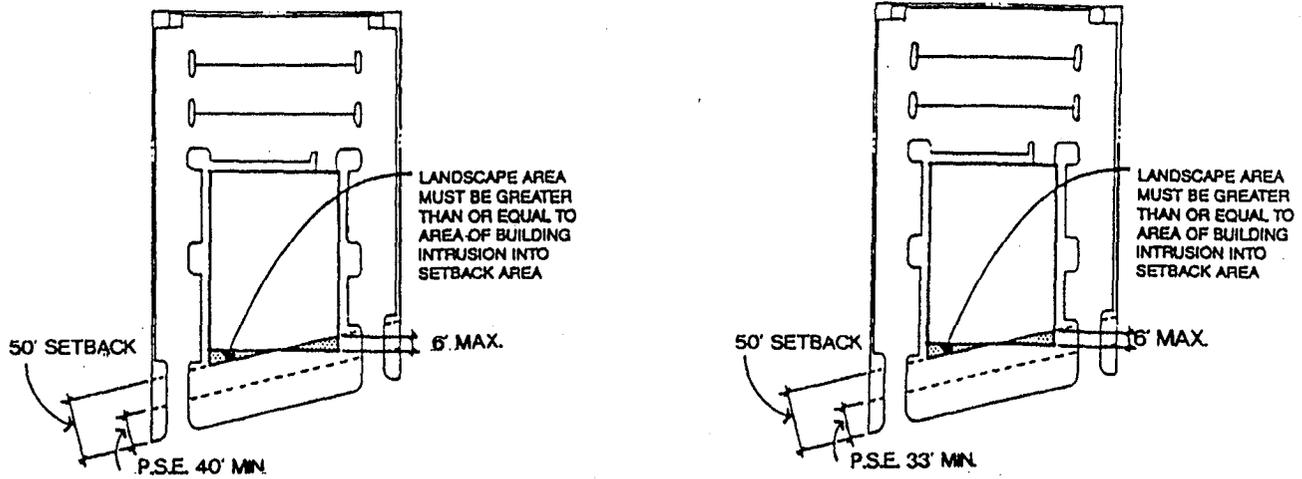
C. Commercial/Retail Building Setback Option

Landscaping shall not be required along a portion of a front and/or side of a commercial/retail building provided that a covered sidewalk with a minimum width of 12' is constructed immediately adjacent to the building and parking is located immediately adjacent to such walk. Small planters or vine pockets are required in the sidewalk area. Those portions of the front and/or side of a building without the covered sidewalk described above shall be landscaped as otherwise required in these guidelines.

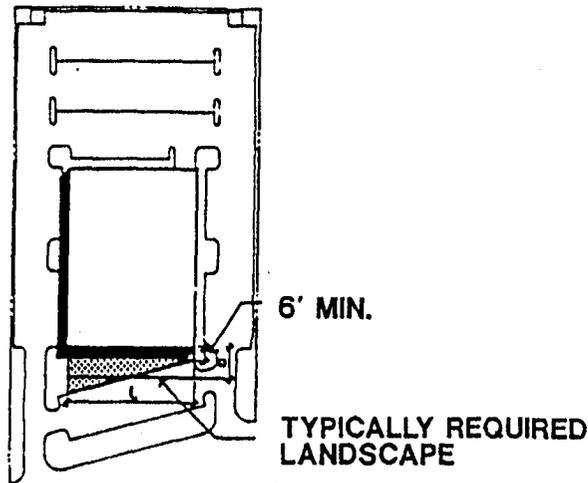


D. Building Setback Exceptions

1. Roof overhangs, buttresses, cornices, and sunshading devices may project into the required landscaping adjacent to a building facade provided these obstructions do not limit the size or density of allowable planting materials and do not project into the setback areas more than 4'.
2. Variable Depth Front Yard: If a building facade is not parallel to the adjacent street, the building may intrude into the setback (measured perpendicular to the building) provided the resulting amount of landscape area is equal to or greater than the area otherwise required. A building may intrude up to 6' into a 50' setback.



3. Variable Depth Landscape Area: For office and retail projects, the depth of the required landscaping adjacent to a building facade shall vary if the landscaping runs the length of the facade and is of an equivalent or greater area to an area of uniform required depth. In no case shall the depth be less than 6' as illustrated below.



PROPOSED AREA  = L x d

2.7 DEVELOPMENT SITE

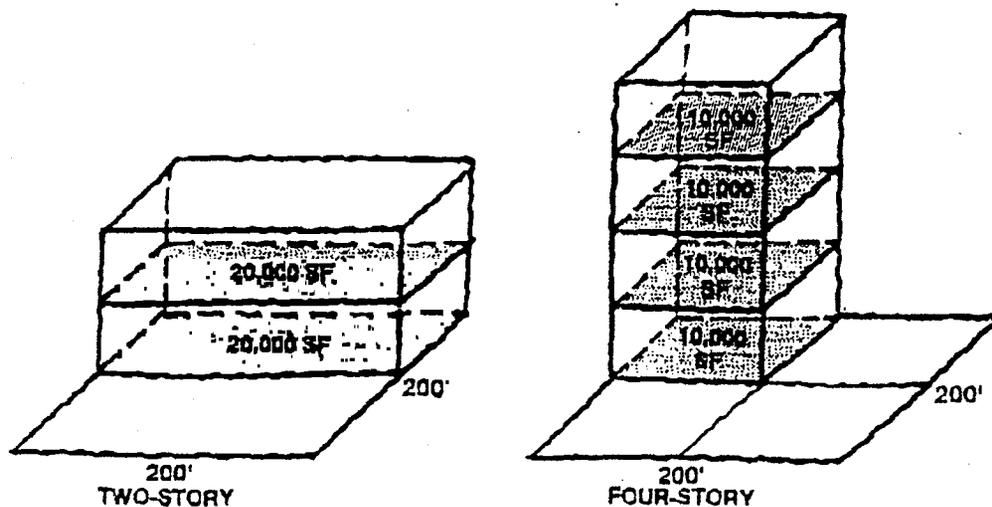
2.7.1 SITE DEVELOPMENT STANDARDS

A. Table 2-C lists the maximum Floor Area Ratio and maximum building height allowed in the Business Park/Light Manufacturing Zone.

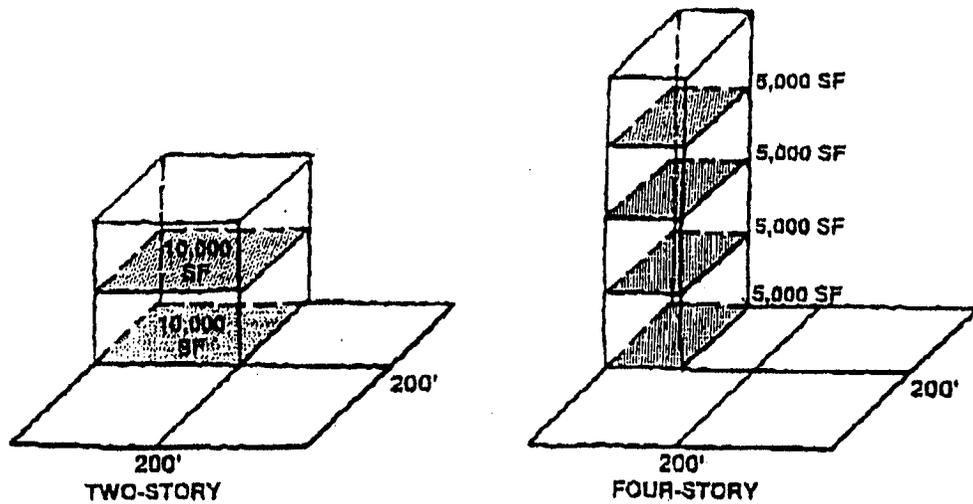
TABLE 2-C
SITE DEVELOPMENT CRITERIA

Land Use	Max. Floor Area Ratio	Maximum Height
Business Park	60%	--
Light Manufacturing	40%	--
Commercial/Retail	30%	30'

B. The Floor Area Ratio (F.A.R.) is defined as the gross square footage of all buildings (excluding basements) on a parcel divided by the gross square footage of the parcel. The resulting percentage must not exceed the maximum F.A.R. established for each parcel listed in Table 2-C. For example, when the gross square footage of all buildings on a site is equal to half of the site gross square footage, the floor area ratio is half, or 50% (also referred to as a .5 FAR). As shown below, achieving a given FAR can be accomplished in different ways by varying the location and height of the building which results in varying arrangement of the building square footage. A 50% FAR can be achieved with a one-story building covering half the site, a two-story building covering one quarter of the site, or a four-story building on one eighth of the site. With the FAR restriction, as the number of stories increases the amount of site area that can be used decreases. Note: Building height, setback and site landscape coverage requirements also influence the location and scale of the building.

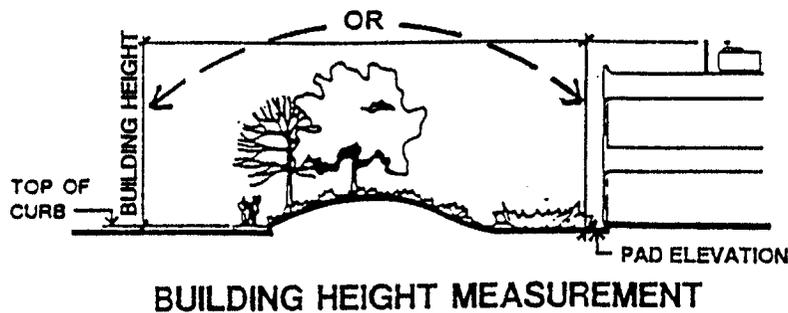


100% or 1.0 FAR – 40,000 SF site; 40,000 SF building



50% or .5 FAR – 40,000 SF site; 20,000 SF building

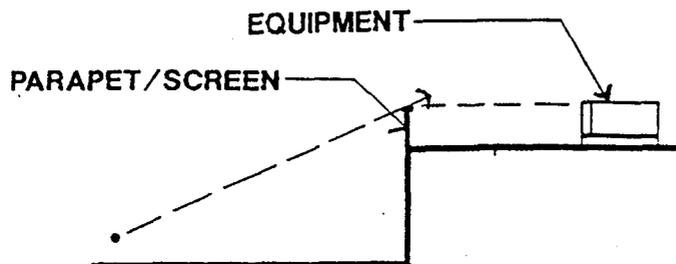
- C. Height is the vertical distance by which any part of a building rises above the top of the highest street curb on or immediately adjacent to the parcel or the vertical distance from the top of the building pad to the highest part of a building, whichever height is greater. The maximum height is stipulated for each planning district in Table 2-C.



2.7.2 ARCHITECTURE

- A. Buildings shall be designed with a high quality visual image, with consistent architectural features on all sides. Buildings shall conform to all current building standards, codes, and requirements. In order to communicate intended architectural quality of the project, developers are required to submit architectural elevations showing all building sides, eye level color perspective renderings, materials samples and a materials color board for The Master Developer Preliminary Plan Review. See Section 8.0 for the Design Review Process.

- B. All building mounted mechanical and electrical equipment shall be entirely shielded from any ground level view with visual barriers of the same or greater height as the objects they are screening. Screens shall consist of architecturally suitable material conforming with the design of the building.



BUILDING MOUNTED EQUIPMENT SCREENING

- C. Main gas meters, pressure regulators, electrical transformers, and fire sprinkler risers shall be located on the side or rear of the buildings and shall be screened by earth mounds and/or landscaping of the same or greater height.
- D. In addition to fire hydrants installed along the streets by the developers, each parcel shall have on-site fire hydrants, as required by the City of Hayward's Fire Marshall.
- E. Prohibited Materials
1. Pre-engineered metal buildings are not permitted in the South of Route 92 Plan Area. Custom designed metal clad buildings may be permitted at the discretion of the Master Developer. Acceptable metal clad buildings include architectural grade siding and should be custom designed rather than a "metal building system".
 2. White reflective gravel shall not be used to build up roofs.

2.7.3 PARKING AREA REQUIREMENTS

A. Minimum Parking Requirements

Required parking ratios are as follows: 1 space/250 S.F. for Business Park; 1 space/350 S.F. for Light Manufacturing and 1 space/200 S.F. for Commercial/Retail uses.

B. Prohibited Parking

No parking shall be allowed on any street except on the south side of Street C nor in access driveways, designated fire lanes, or in violation of preferential parking restrictions. The Master Developer shall have the right, but not the obligation, to enforce this section by causing the removal of any vehicle parked in violation of this section. This section shall not be construed to prohibit the parking of emergency or public utility vehicles, or vehicles associated with the construction of new buildings. Each owner shall designate fire lanes and other no-parking or restricted parking areas on a parcel as required by Section 4 of the City of Hayward Off-Street Parking Regulations, July 16, 1996 and the California Vehicle Code

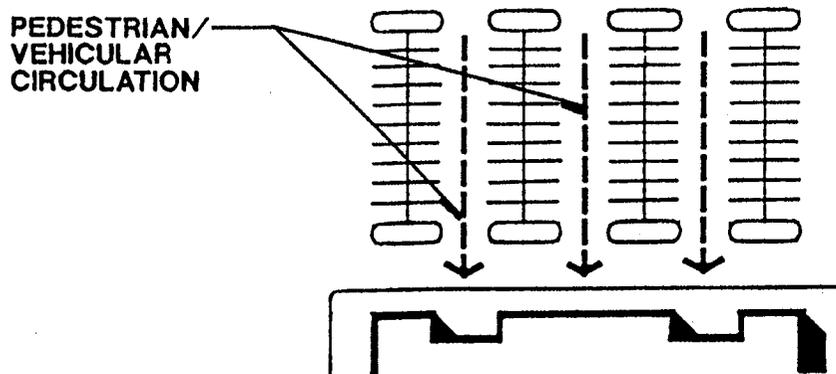
to permit citation or removal, or both, of vehicles parked in violation of the provisions of this section.

C. Parking Lot Drive Aisles

All parking areas shall provide adequate driveway space for the movement of vehicles with a 26' minimum width drive aisles, except at common vehicular access drives. See Section 2.5.D.

D. Circulation, Fire Lanes, and Special Access

1. No parking shall be allowed on any public street in the South of Route 92 Plan Area, except on Street C and Street E. Parking lot circulation shall be contained within each parcel, except sites where common driveways use two parcels. In no case shall the adjacent street be used as a drive aisle for parking lot circulation.
2. Parking location and layout should facilitate convenient and safe pedestrian circulation. If parking areas exceed one double row in depth, it is preferable that the alignment of the aisles be in the direction of the pedestrian movement.



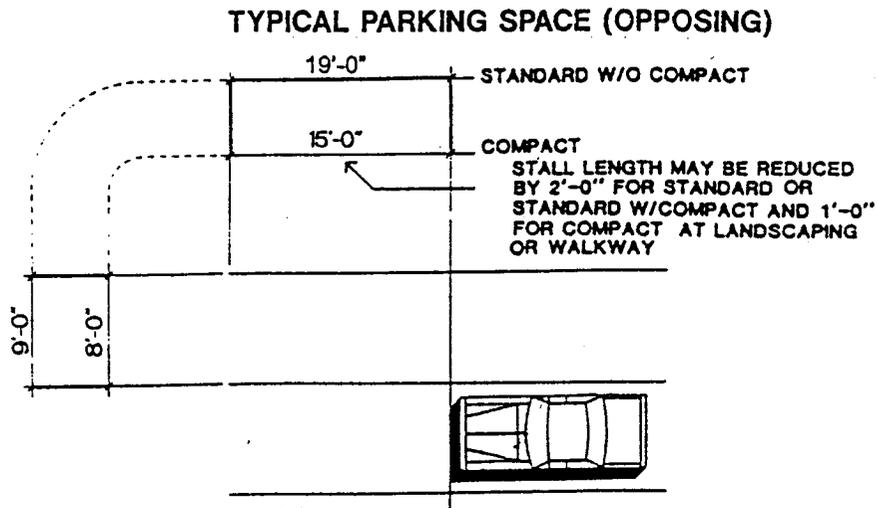
3. All fire lanes and other no parking or restricted parking zones, lanes or areas shall be signed, curbs painted red, and pavement striped in accordance with requirements of the Hayward Fire Department, and as required by the California Vehicle Code.
4. Current requirements for United States mail and delivery should be coordinated with the U.S. Postal Service.

E. Paving and Striping

1. Parking areas shall be paved to City street standards with asphalt or Portland Cement Concrete so as to provide all weather surfaces.
2. Parking areas should be striped and each space designated with white painted lines on the paved surfaces.

F. Parking Stall Requirements

1. The number of on-site parking spaces required for each land use is listed in Section 2.7.3.A and is a specified amount based on a calculation using the gross floor area ratio and landscape area ratio.
2. All required parking spaces shall be the equivalent of the 90° opposing stall and corresponding drive aisle dimensions as shown in the following illustration.
3. The maximum number of compact stalls is 30% of the total number of stalls required for on-site parking.



**TABLE 2-D
TYPICAL PARKING STALL SIZES**

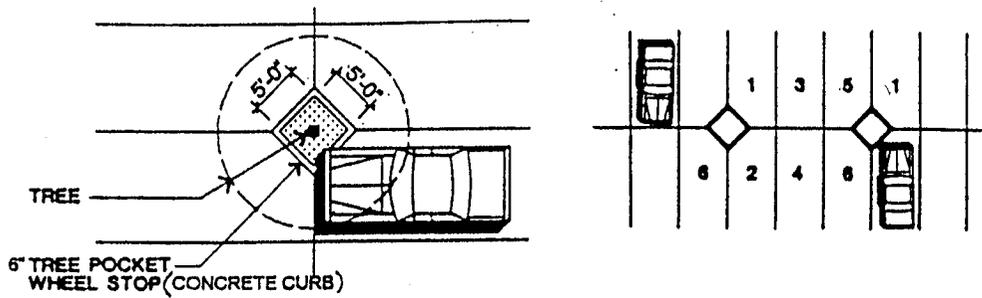
	Stall Length	Stall Width	Overhang Allowed	Length of Reduced Stall
Standard	18'-0"	9'-0"	2'-6"*	16'-6"
Compact	15'-0"	8'-0"	2'-0"	13'-0"

* The area of overhang is not included in required landscape setbacks.

4. As an alternative, angled parking will be permitted provided that the Master Developer finds all of the following objectives are not met with 90° parking: ease of site circulation, ease of stall maneuverability, and traffic safety. While geometries may vary, tree quantity and placement requirements set forth in Section 2.7.5 shall still be met.

F. Parking Lot Tree Pockets

For Business Park/Light Manufacturing sites, between each group of six 90° opposing parking spaces there shall be at least one diamond tree pocket wheel stop surrounded by a 6" concrete curb. Where there is more than one double row of parking, other geometries may be approved



at the discretion of the Master Developer if they provide at least 25 square feet of planting area and at least 5' clear planting area (inside of curbs) in all directions. Commercial/Retail projects may have up to a group of eight 90° opposing parking spaces per one diamond shaped tree pocket. Refer to Section VII, Article 2, City of Hayward Off-Street Parking Regulations, July 16, 1996.

H. Handicapped Parking Requirements

Parking spaces for the handicapped shall be located near building entrances in conformance with the current federal, state and local codes in effect at the time of construction for each building. Refer to Article 2, City of Hayward Off-Street Parking Regulations, Section VII.

I. Preferential Parking

Reserved preferential parking spaces shall be designated for ridesharing carpools or vanpools. The number of preferential parking spaces designated shall be a minimum of 5% of the total number of automobile stalls required by the City code or 5% of the employees at maximum shift, whichever is less. Parcel owners shall install a minimum of one sign per two spaces and paint pavement designations as indicated below. Refer to Section IV, Article 10-2.403 in the City of Hayward Off-Street Parking Regulations, July 16, 1996.

J. Bicycle Racks

All Business Park/Light Manufacturing, and Commercial/Retail parcel owners shall provide, at each building, a minimum number of bike racks or lockers to accommodate the equivalent of 3.5% of the total number of automobile stalls required. Bicycle racks shall be located in a convenient, secure location; shall provide the ability to lock the bike to the rack; and shall preferably be sheltered from the sun and rain. Each parcel owner may determine the type of system to be installed.

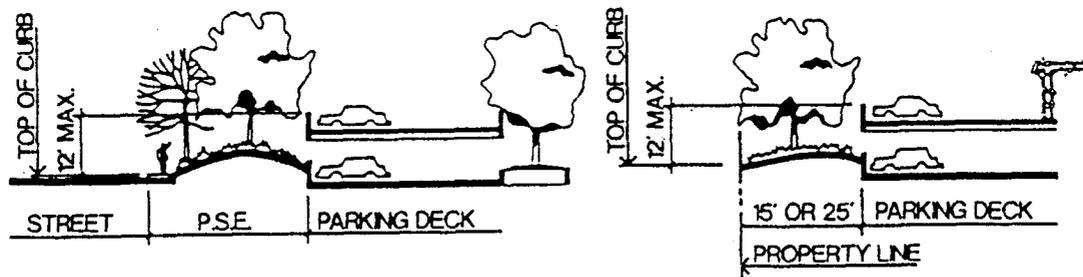
K. Parking Decks

Single level parking decks lower than 12', measured from the top of the rail of the upper deck to the top of the highest street curb on or immediately adjacent to a parcel, may be permitted at the discretion of the City of Hayward in front yards and may be contiguous with the interior boundary of the PSE. They shall be located a minimum of 25' from the side property line and

shall be a minimum of 15' from the rear property line.

Parking decks shall be screened from the street and adjacent parcels by the use of plant materials and/or architecturally designed to integrate the parking decks with the adjacent development. There shall be a minimum of 9' of landscape required immediately adjacent to all elevated parking deck elevations not fronting on a landscaped area of the same width or greater

In addition, open roof decks shall be planned to visually break up large surfaces with either an equivalent area of landscaping required for on-grade parking surfaces in the form of planting at the deck perimeter, shrub pockets on the deck surface, trellises with vines for screening cars, trees where structurally practical or by other architectural means. Stall and aisle dimensions for deck parking shall be the same as for those on grade.



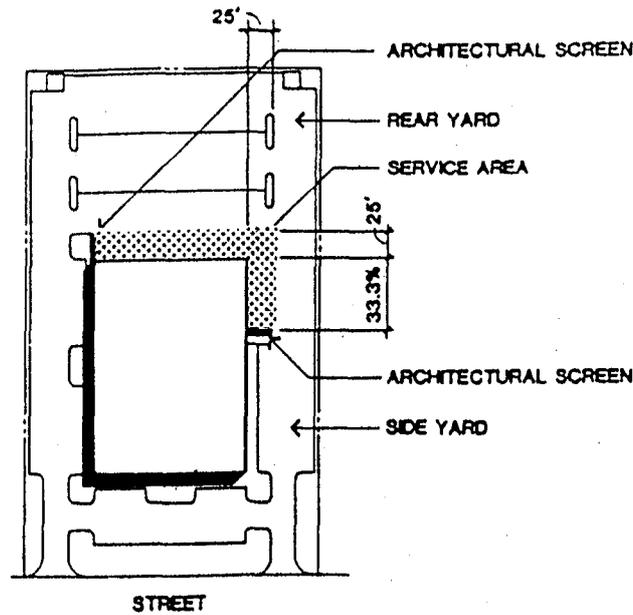
FRONT YARD PARKING DECK

REAR OR SIDE YARD PARKING DECK

Multiple level parking structures (i.e. higher than 12' or more than one story above curb grade) shall be considered buildings for the purpose of establishing setbacks. The area of single or multiple level parking structures shall not be included in the determination of Floor Area Ratios.

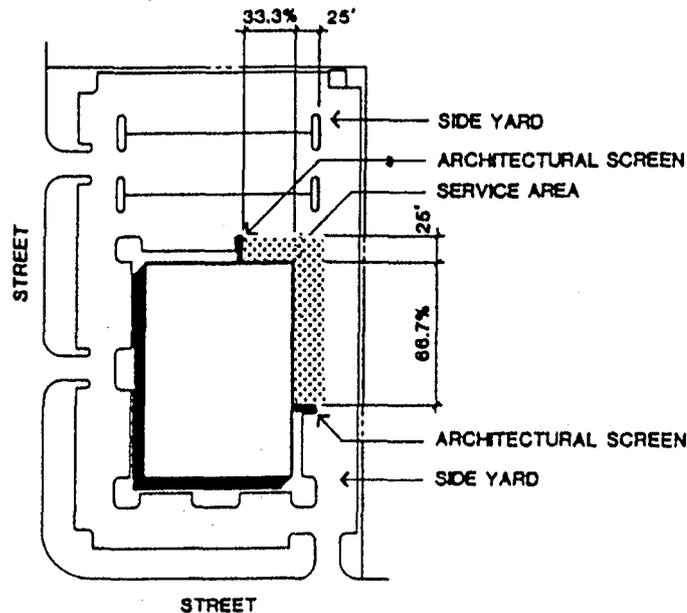
2.7.4 SCREENING OF SERVICE AREAS, UTILITIES AND LOADING AREAS

- A. Light Manufacturing/Research and Development areas: Service areas, which may include truck facilities, materials, supplies, and exposed mechanical and electrical equipment, must be screened with visual barriers so they are not visible from public streets. In addition, materials and supplies must be screened from adjacent parcels. The maximum width of service areas in side yards shall be 25' and service area screens shall be of architecturally suitable materials, no lower than 8' high, no higher than 10', and may extend from the building to the drive aisle.
- B. Commercial/Retail and Office: Service areas shall be permitted, but shall be architecturally screened in their entirety. The size and location must be approved by the Master Developer.
- C. Interior Parcel All Districts: The service area shall be architecturally screened and is restricted to rear yards and/or no more than the rear one third of building length of one side yard.



INTERIOR PARCEL SERVICE AREA

- D. Corner Parcel All Districts: The service area shall not extend beyond the rear two thirds of building length on one side yard nor the rear one third of the other side yard.



CORNER PARCEL SERVICE AREA

- E. Outdoor Refuse Collection Areas: All outdoor refuse collection areas shall be visually screened by enclosures which may extend to the property line and should be approximately 8'-0" in height. The screening shall consist of suitable architectural materials compatible with the building and shall have gates. The enclosure shall be protected by 3'-4' concrete wall pipe bollards or concrete curbs used to confine movement of the dumpsters.

Land uses (excluding commercial/retail uses) should have approximately one dumpster per 75,000 gross square feet of building area. Variations to this quantity may occur when a trash compactor is used, or when pick-up service is more than once a week.

Collection areas must be situated to provide clear and convenient access to refuse collection vehicles but cannot be located between a street and a building. The intention of this guideline is to ensure that all refuse on-site is contained and screened from public view.

- F. **Loading Areas:** No truck loading area, whether a truck dock or at-grade truck door, shall be located in the front of any structure, within any frontage setback area, or between the front of any structure and the front property line. Further, no such trucking facilities shall be located on a structure in such a manner that said facilities are visible from a street. Trucking facilities may be located on the sides of a building or other structure provided said facilities are screened from view from a street. Notwithstanding the foregoing, trucks may be parked temporarily in the front of any building located on a parcel in an office or commercial/retail planning district, designated CR, BP, LM or by the applicable zoning ordinance of the City, for the purpose of unloading supplies or equipment consigned to any business in said building if, and only if, said building has no loading area to the side or rear of said building.
- G. **Outdoor Storage:** No outdoor storage shall be permitted unless completely screened from public streets. Large quantities of raw materials or partially assembled components are prohibited. All commercial/utility vehicles stored on-site must be inside a closed building or within a screened portion of the parcel.

2.7.5 LANDSCAPE ELEMENTS

This addresses all exterior elements within a development site and excludes the Public Service Easements and building area. Common concerns within this area include: landscaping, irrigation, parking lot layout, and drainage.

A. Parking Area Landscape Requirements

1. A minimum of one 15 gallon tree is required per each six parking stalls (eight parking stalls at Commercial/Retail sites) to be placed at intersections between or immediately adjacent to the stalls. A combination of diamond tree wells providing an area 5' square and a 6" concrete curb, planter islands, or planting areas within 6' of the parking lot may be used to achieve the minimum tree quantity.
2. Tree wells and planters within paved parking areas shall provide a minimum 4' clear planting space. Prior to planting, tree wells shall be prepared to insure adequate relief from soil compaction incurred in the construction of the parking lot.
3. The ends of parking rows will be capped with landscaped medians.
4. Planting edge curbs along parking areas should be used in lieu of wheel stops. Class B Portland Cement curbs, 6" above pavement will be constructed where any landscaped area adjoins parking area or driveways.

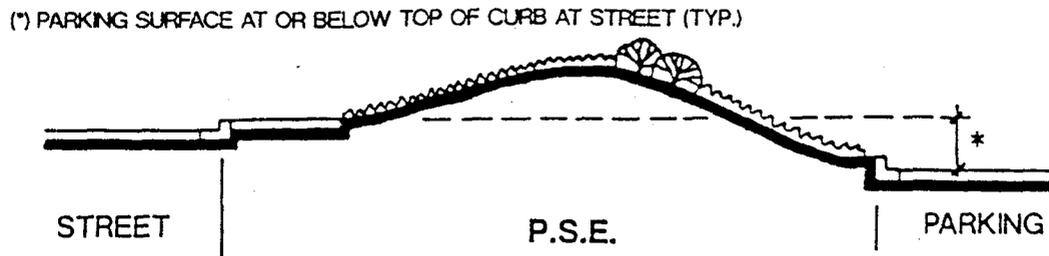
5. The tree species are described in Section 2.7.7 A planting drainage test is recommended in selecting the correct species. If special soil or parcel conditions prevent the use of trees from the list, the parcel owner should recommend alternatives to the Master Developer.

B. Side and Rear Yard Requirements

1. A 5' minimum planting strip is to be provided continuously along and adjacent to interior property lines so that where two properties adjoin, there shall be a 10' zone. See Section 2.5 for common drive exception.
 2. A maximum slope of 3:1 is allowed within the 5' planting zone. A one foot minimum flat transition area shall be provided at the top and bottom of all slopes within this zone. Thus, a maximum of 18" change in grade is possible within the 5' zone.
 3. Either shrub and/or tree plantings shall be required in the 5' planting zone. Where two parcels adjoin, to achieve proper screening shrub and tree planting the first property owner to receive plan approval may plant a combination of trees and/or shrubs. The second property owner then must plant the required compliment of trees and/or shrubs.
 4. Landscaping adjacent to a common access drive, as defined in Section 2.5, shall be coordinated so that both sides of the drive feature the same plant materials and include small to medium shrubs.
 5. No walls or fences exceeding 30" in height shall be permitted within a required 5' planting zone. Walls or fences separating adjoining parcels may be permitted when located at the property line, at the discretion of the Master Developer.
- C. All landscape areas within each parcel shall be planted and irrigated in accordance with the planting palettes and criteria recommended in these guidelines and the Hayward Water Efficient Landscape Ordinance.

2.7.6 GRADING AND DRAINAGE

- A. Building pad elevations shall be set no lower than the median elevation of the surrounding curb on the public street on which the property fronts.
- B. Where proposed on-site parking is immediately adjacent to a public street, the finished pavement grade in the parking area at its perimeter curb shall be at or below the lowest elevation of the existing street top of curb elevation adjacent to the parking.
- C. All sites shall provide an on-site storm drainage collection system which is connected to the public storm drainage system. Sheet drainage across property lines and over driveways into streets is prohibited. Drainage must meet City standards for non-point discharge.



- D. No excavation shall be made on and no sand, gravel, soil or material shall be removed from any parcel without approval of the Master Developer, except in connection with the development of a site. Upon completion of such construction on a site, exposed openings shall be backfilled to grade, and disturbed ground shall be graded level and paved or landscaped.

2.7.7 PLANTING

- A. These Development Guidelines are not intended to identify specific plants but rather to suggest the shape and intended function of the streetscape.

The use of specific plants will be subject to direction provided in the following city publications and review and approval by the City Landscape Architect.:

1. STREET TREES, Section 7-2.50 through Section 7-2.65
2. STREET TREE PLANTING SPECIFICATIONS, June 15, 1993, Drawing no. SD-122
3. LANDSCAPE DESIGN CHECKLIST, pages 1-6
4. OFFICIAL STREET TREE LIST, as prepared by the City.
5. HAYWARD LANDSCAPE BEAUTIFICATION PLAN, Oct. 1987, prepared by POD, Inc.
6. HAYWARD WATER EFFICIENT ORDINANCE

All of the above documents are on file and available in the Offices of the City Clerk and the Department of Public Works Development Review Services Division.

- B. Plant Material Installation

All 5 gallon size trees shall be single or double staked and all 15 gallon and box size trees shall be double staked as specified in the City's Standard Detail 122. Trees shall be secured with corded rubber ties or V.I.T. rigid tree ties. Trees shall be tied to the stake in at least two places on species where extra support is required to maintain a straight trunk.

1. Trees larger than 24" box size are recommended to be secured with guy wires and reinforced rubber hose guards. Guy wires in lawn areas shall have 1/2" white P.V.C. sleeves.
2. Lawn areas shall be no less than 8' in width if irrigated with conventional spray systems.

C. Irrigation

1. All new or rehabilitated irrigation systems shall meet the requirements of the City's Water Efficient Landscape Ordinance.
2. All landscape areas shall be fully irrigated with an automatically controlled underground irrigation system.
3. Landscape irrigation and domestic water services to each site shall be separately metered. Each irrigation service point of connection shall have a backflow prevention device installed above grade per City of Hayward's requirements. Devices shall be screened with planting.
4. The irrigation system shall be designed and installed to be compatible with the possibility of a future reclaimed water system. Special mainline pipe coding, quick couplers, and valve tags shall be installed per current State Code for reclaimed water irrigation systems.
5. All irrigation heads within a Public Service Easement, common area, or next to walkways or parking shall be pop-up type. No fixed risers are permitted in these areas.
6. Irrigation systems shall be designed to be generally limited to stream rotor or low gallonage spray heads for turf and groundcover, and drip/bubbler for trees and shrubs.
7. The irrigation system shall be designed to minimize spray onto non-planted areas.
8. Precipitation rates for sprinkler heads and individual valve timing shall be no greater than the soils absorption rate. Irrigation heads shall have matched precipitation rates within each control valve circuit. The application rate shall not exceed 0.75" per hour.
9. The amount of irrigation water applied shall be reduced after plant materials have become established and shall be adjusted each season to account for cooler temperatures as well as natural irrigation provided by rainfall.
10. Gate valves shall be installed to allow for shutting down various sections of the system for service and repairs without affecting the entire system.
11. All piping shall be P.V.C. buried to a minimum depth of 12" (lateral lines) or 18" (pressure lines).
12. Install anti-drain valves (check valves) in strategic points along lateral line or at sprinkler heads to prevent erosion and water loss from "low" elevation sprinkler heads.
13. Landscaping shall be watered only between the hours of 10:00 P.M. and 6:00 A.M.

2.7.8 SITE MAINTENANCE

- A. All areas of each parcel not used for structures, walkways, paved driveways, parking or storage areas shall be maintained, at all times, under the supervision of a professional landscape maintenance contractor or certified gardener, in a fully and well-kept landscaped condition. This shall include regular mowing, trimming, fertilizing, pest control, plant replacement and clean-up.

- B. The owner of each parcel shall at all times properly maintain and keep the entire parcel, including all improvements, in a safe, clean and sightly condition, in a good state of repair, and shall comply in all respects with all governmental, health, fire and police requirements and regulations.
- C. The owner of each parcel shall, at his own expense, remove rubbish of any character whatsoever which may accumulate on such parcel.
- D. Each owner shall be responsible for the cleaning, maintenance, and relamping of any external lighting fixtures.
- E. Undeveloped parcels shall be maintained in a neat, weed-free condition by the Master Developer.

SECTION 3 - RESIDENTIAL
DEVELOPMENT

3.0 RESIDENTIAL DEVELOPMENT

3.1 PLANNING CONCEPT

The residential enclave within the South of Route 92 Plan Area includes buffered open space areas on over 60% of its periphery. Access is gained via an overpass spanning the Union Pacific Railroad right-of-way through the light manufacturing area offering views of the neighborhoods, nearby wetlands and distant bay.

Three subdivisions on 5,000, 6,000 and 8,000 square foot lots are planned. All of the 578 housing units will be single family detached of mixed one and two story floor plans.

The layout of some of the streets and the siting of some of the homes is oriented toward the bay. Two neighborhood parks within the enclave are linked by a bicycle and pedestrian trail system providing easy access for residents.

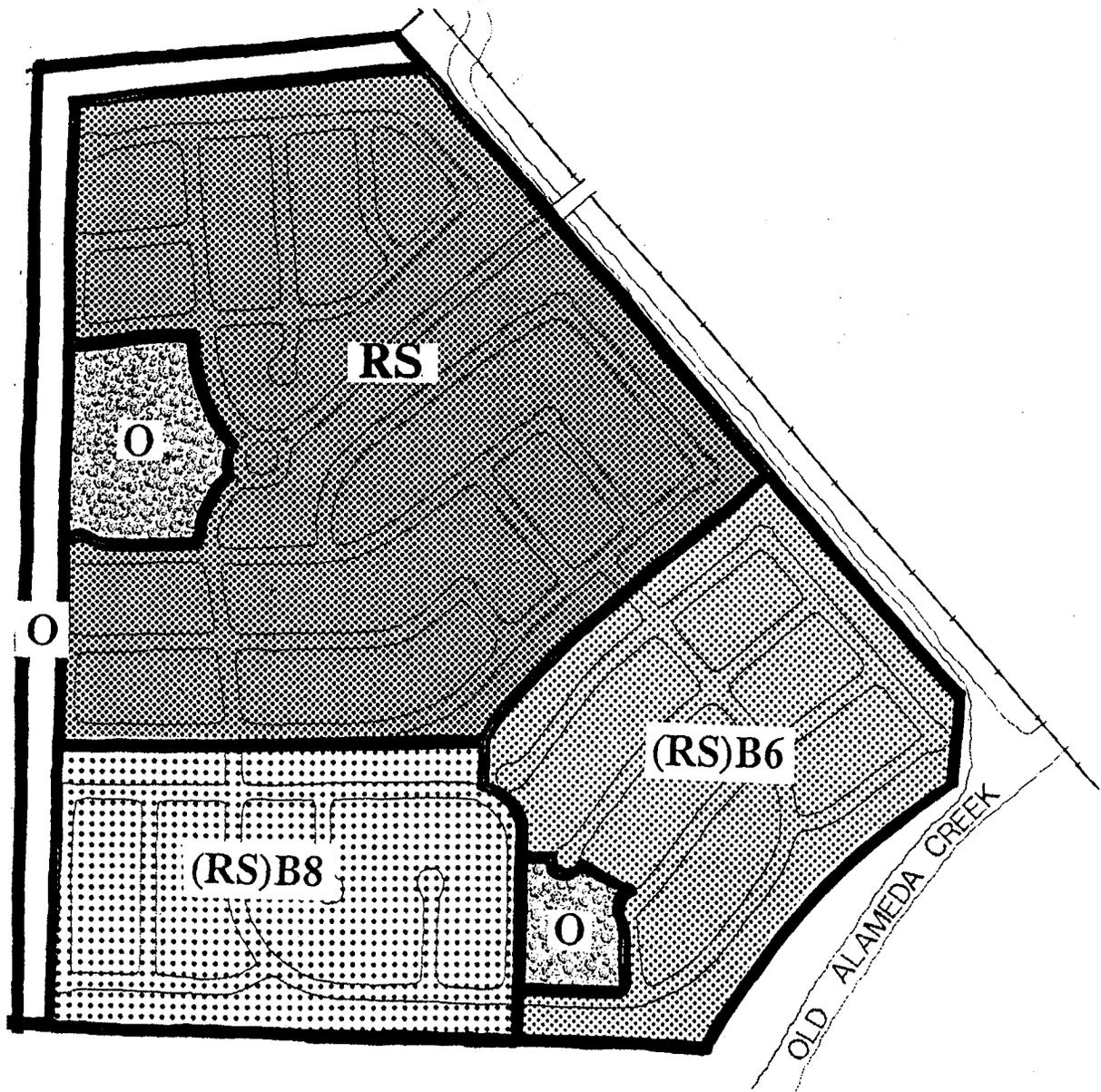
Homeowners Association(s) will be established; each will have professional management.

**TABLE 3-A
SUMMARY OF LAND USES**

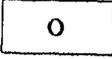
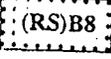
	Business Park	Residential	Light Manufacturing	Retail	Parks	Roads	Landscape/ Buffer/ Wetlands	Total Acres
Oliver East	42.1		27.6	3.5	23*	10.1	2.0	108.3
Oliver West	--		--	--		26.1	15.4	130.5
City of Hayward	12.0		--	--	--	0.2	--	12.2
Weber	--		21.5	--	--	2.0	57.0	80.5
ACFCWCD	--		--	--	2.0*	--	--	2.0
Total	54.1		49.1	3.5	32.5	38.4	74.4	333.5

* Sports Park

Source: Ruggeri-Jensen & Associates, Land Use Plan, South of Route 92 Specific Plan, June 27, 1997. Economic and Planning Systems, Chapter X, Financing and Implementation, South of Route 92 Specific Plan, Draft, August 15, 1997



LEGEND

- | | | | |
|---|---|--|----------------------------------|
|  RS | RESIDENTIAL-SINGLE FAMILY
5,000 SQ. FT. LOTS |  O | OPEN SPACE-
NEIGHBORHOOD PARK |
|  (RS)B6 | RESIDENTIAL-SINGLE FAMILY
6,000 SQ. FT. LOTS |  O | OPEN SPACE-
BUFFER ZONE |
|  (RS)B8 | RESIDENTIAL-SINGLE FAMILY
8,000 SQ. FT. LOTS | | |



OLIVER WEST RESIDENTIAL ZONING PLAN

Exhibit 3-A

Residential Development

Development within the South of Route 92 Residential Plan Area neighborhoods shall conform to the zoning established on the Zoning Plan. See Exhibit 3-A. Specific uses allowed by zoning are set forth in Section 3.2.

The Residential Zoning Plan illustrates the variety and location of residential subdivisions, lot types and sizes, parks, pedestrian and bicycle trails, public spaces, open space, and buffer areas located adjacent to or within the various neighborhoods.

3.2 RESTRICTIONS ON USE

The use within each residential subdivision shall be in compliance with the uses as illustrated on the Zoning Plan.

Permitted uses within the South of Route 92 Residential Plan Area are as follows:

A. Residential Development

Single family detached homes within subdivisions on individual lots of 5,000 square feet or more.

B. Public Facilities

1. Neighborhood parks
2. Recreational facilities
3. Trails; pedestrian and bicycle
4. Wetlands
5. Habitat
6. Buffer Areas.

3.3 LANDSCAPE CONCEPT

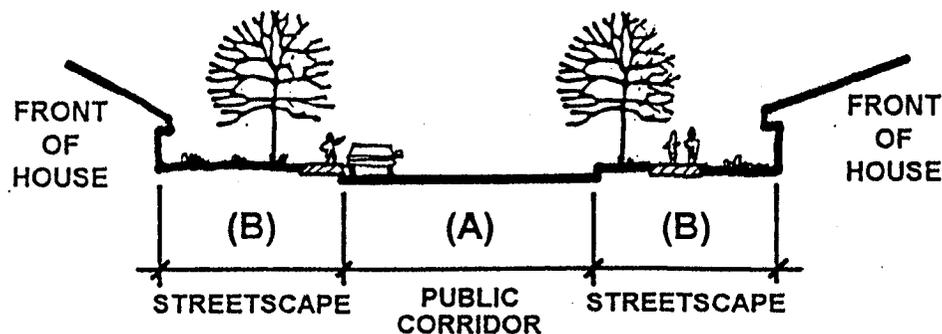
A. The overall concept for the development of public rights-of-way within the South of Route 92 residential area is to establish an attractive image for the neighborhoods based on the direction set within the Business Park. This will include:

1. Creation of special residential village monumentation elements in the westerly two quadrants of the Street A and Street B intersection
2. Establishment of a unique tree selection for use along Street A to its termination at the east roundabout. Selection of unique trees for each residential street and neighborhood
3. Distinctive landscape treatment of the center of the roundabout
4. Provision of a landscaped parkway linking the two neighborhood parks
5. Creation of vertical tree masses on either side of the 5 acre neighborhood park defining the westerly view of wetlands and distant bay
6. Creation of selected openings in the streetscape to allow view into the landscaped buffer zones.
7. Distinctive landscaping, reminiscent of historic flower production, on the slopes of the overpass, in the roundabout and along the parkway.

- B. The Landscape Master Plan illustrates the features and intent of the landscape concept. For the purposes of these guidelines the South of Route 92 Residential Plan Area consists of two distinct zones: 1) The PUBLIC CORRIDOR and 2) The STREETScape. In general, the Public Corridor and the Streetscape have been planned to unify the South of Route 92 residential area.

3.4 DEFINITIONS

The residential portion of the South of Route 92 Plan Area is located on the Oliver West parcel. For the purposes of these guidelines, this portion consists of four distinct areas: the Public Corridor, the Streetscape, the Parks and Trails and the Open Space. The guidelines for all of these areas will unify the plan area and ensure that it is a high quality residential development. The location, definition of and responsibility for construction and maintenance are illustrated in the diagram below and discussed in the following text.

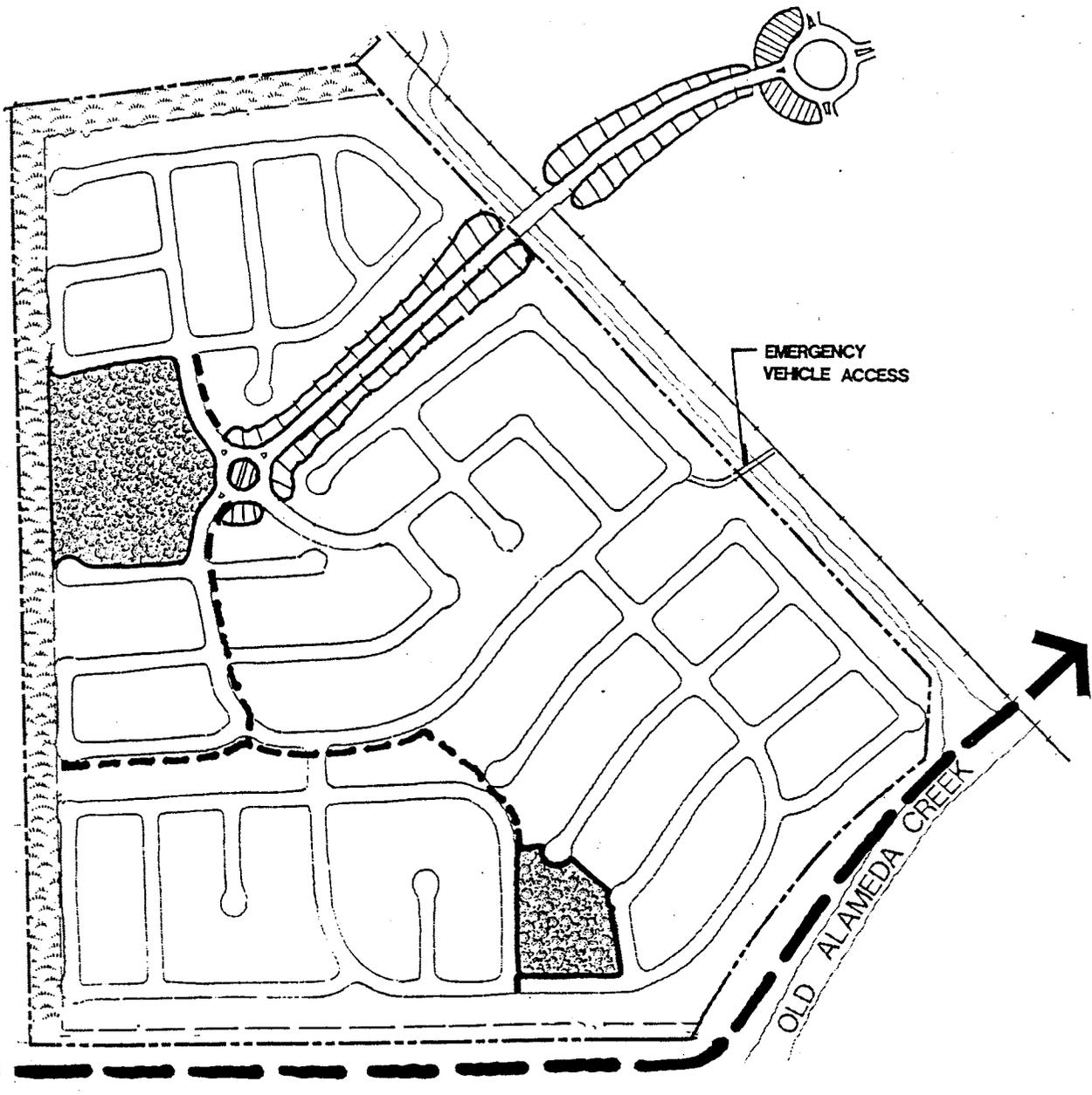


- A. The Public Corridor is shown in the street sections which follow. It extends from the property line at back of curb to the property line at back of curb.

The Master Developer will construct the Public Corridor elements and the City of Hayward will maintain them, except for any private streets. The Public Corridor includes all public or private streets, and roundabouts.

The Streetscape extends from the back of curb for 20' and contains:

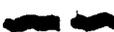
1. Sidewalks, off-street bike paths, street lighting, street furniture, project signage, and bus stops. Bus stops may include a concrete pad, benches, shelters and lighting. These elements are to be constructed and maintained by the a homeowners association.
2. An easement for trenched utilities. The easement is to be maintained by the homeowners association and the utilities by the utility providers.
3. Project gateways and monumentation including walls, landscaping, lighting and irrigation.
4. Street trees. Trees to be planted by the a homeowners association and maintained by each homeowner.



EMERGENCY
VEHICLE ACCESS

OLD ALAMEDA CREEK

LEGEND

-  NEIGHBORHOOD PARK
-  BUFFER ZONE
-  OVERPASS TO RESIDENTIAL VILLAGE
-  ROUNDABOUT
-  BAY TRAIL
-  NEIGHBORHOOD TRAIL
-  RESIDENTIAL ENTRY

OLIVER WEST LANDSCAPE MASTER PLAN

Exhibit 3-B

Residential Development

- C. Parks and Trails includes the neighborhood parks and the perimeter and internal trails. These will be constructed by the Master Developer. The parks will be maintained by the City and the trails by the Master Developer.

Open Space includes the edges adjoining wetlands and the buffer. Treatments for both will be provided and maintained by the Master Developer.

3.5 PUBLIC CORRIDOR

3.5.1 PUBLIC STREETS AND ROUNDABOUTS

A. Community Collector

Street A provides a means to traverse the South of Route 92 Plan Area and access the residential neighborhoods. In keeping with this roadway's role, an enriched landscape treatment is envisioned.

B. Local Streets

1. Local streets in residential neighborhoods shall be tailored to that residential community and have a variety of landscape treatments. Local roadways in individual neighborhoods may be curvilinear streets ending in cul-de-sacs or loops or traditional grids.
2. Sidewalks are required on both sides. Parking may be on one or both sides and street right-of-ways are 46' with a travelway of 36'.
3. Local public streets which are less than 600' between intersections may have a right-of-way of 60' or less, whether the street ends in a cul-de-sac or not.

C. Roundabout

The extension of Street A serves as the main collector into the residential site. The first intersection after the railroad overpass will have a "roundabout" configuration.

To accommodate the California Design Vehicle (5-axle, 65-foot long truck), a 110-foot wide inscribed diameter roundabout would be required in a residential area. The minor arterial would approach the roundabout at 75 to 90° to the main collector.

Traffic control devices shall be installed to facilitate a smooth one-way circulation in the roundabout, as follows:

1. Channelization including painted triangular islands at entrances to the roundabout.
2. One way signs
3. Yield signs
4. An edge line 5' from the inside circle island.

10' wide crosswalks shall be installed across the respective approaches. A limit line shall be placed four feet in advance of the crosswalk for traffic approaching the roundabout.

On-street parking shall be prohibited on the roundabouts and on adjacent streets 50 feet from the edge of the roundabout's outer circle to assure adequate visibility. Thus, those lots immediately adjacent to the roundabout on both sides of the street may not have on-street parking.

3.6 STREETScape

- Streetscapes are only to be used for landscaping, walkways, driveway access to public streets, connections to public utilities, underground utilities, slope easements, neighborhood and builder parcel entries, and directional signage. All of the prescribed Streetscape improvements are to be installed during building construction. Unless Streetscape improvements are common areas, they shall be maintained at the expense of the parcel builder. Development of the Streetscape shall be in accordance with the applicable landscape guidelines.

**TABLE 3-B
RESIDENTIAL PROPERTY LINE SETBACK CRITERIA**

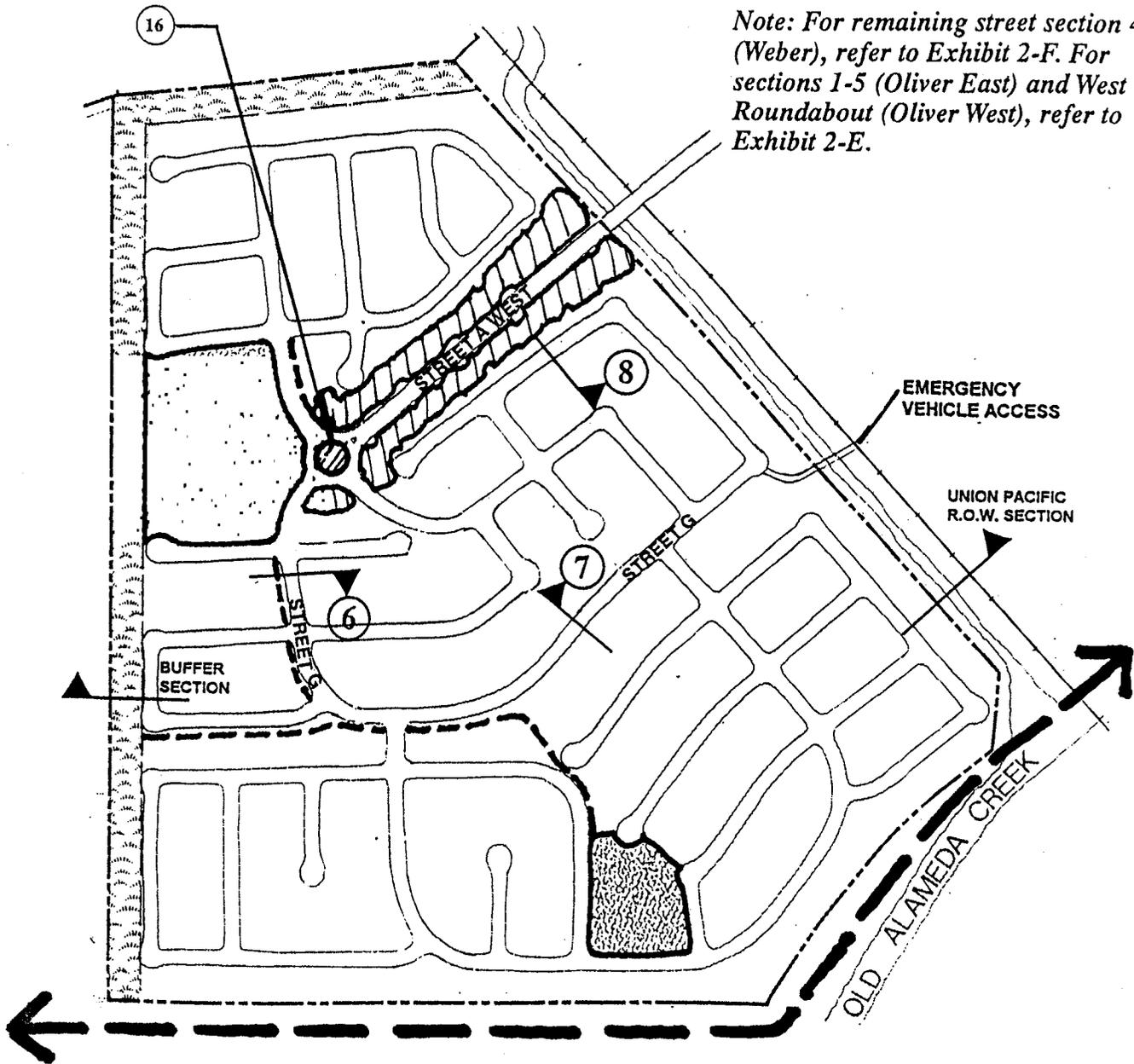
Roadway Category	Min. Front Yard Setback	Min. Side Yard Setback	Min. Rear Yard Setback
Residential-Collector (Street G)	30'	5'*	20'
Residential - Street (typical)	20'	5'*	20'

* 5' or 10% of lot width at front setback line whichever is greater, up to 10'

Note: 1) Road criteria based in accordance with the City of Hayward "Standard Street Sections" for public streets. 2) All front yard setbacks are taken from back of curb to front of structure.

Different street character types make up the Streetscapes in and around the South of Route 92 Residential Plan Area, including local arterial roads, Street A and two levels of local streets. The first level consists of a 25' minimum parkway on one side that includes the neighborhood trail system. The second level has two 5' PSE on both sides. These two Residential street types have 40' R.O.W.s and 36' of pavement with parking on both sides.

Note: For remaining street section 4 (Weber), refer to Exhibit 2-F. For sections 1-5 (Oliver East) and West Roundabout (Oliver West), refer to Exhibit 2-E.



LEGEND

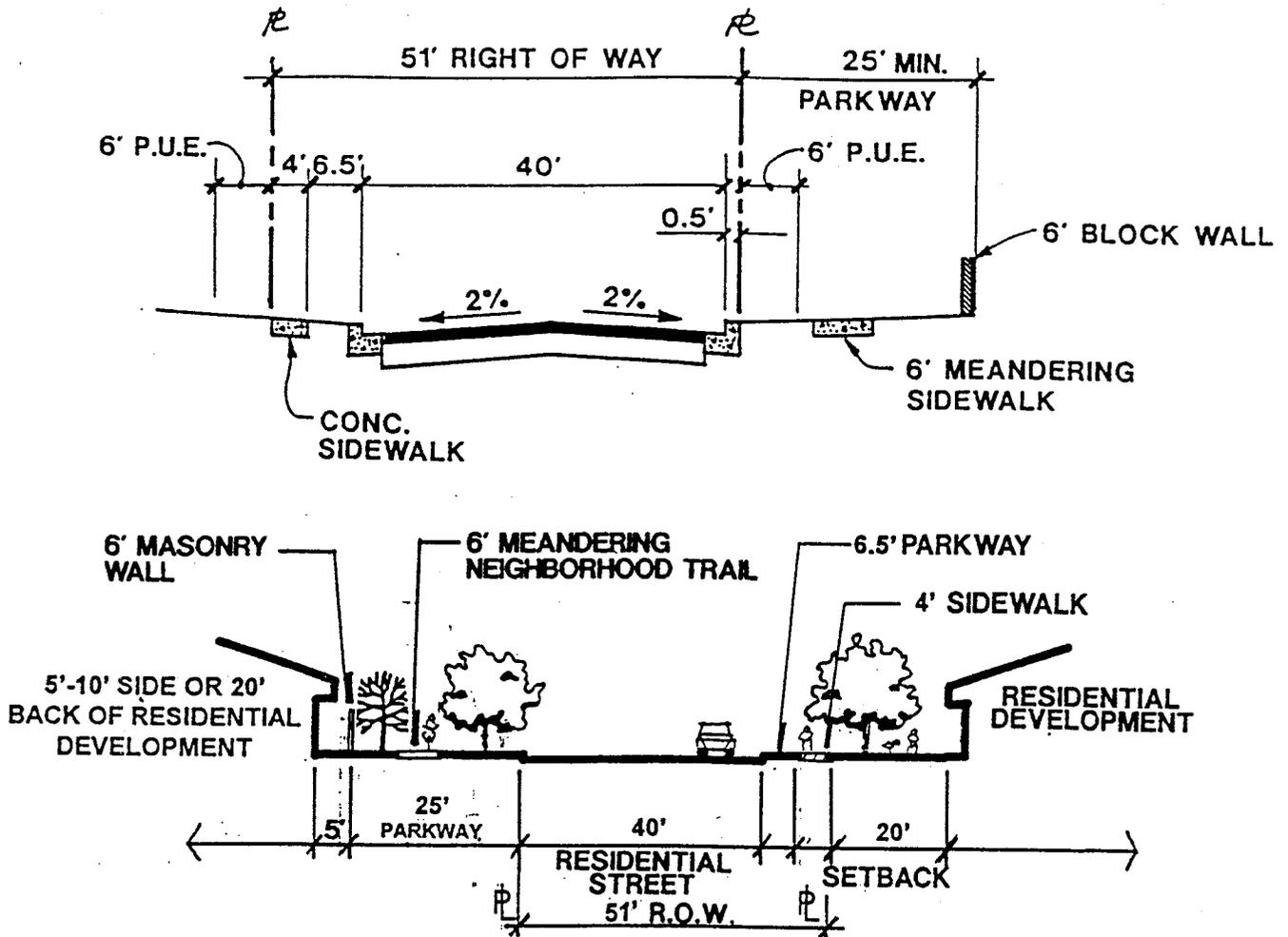
- ⑥ STREET G-RESIDENTIAL COLLECTOR WITH PARKWAY
- ⑦ STREET G-RESIDENTIAL COLLECTOR
- ⑧ LOCAL RESIDENTIAL STREET (TYPICAL)
- ⑬ WEST ROUNDABOUT

OLIVER WEST RESIDENTIAL CROSS SECTION KEY

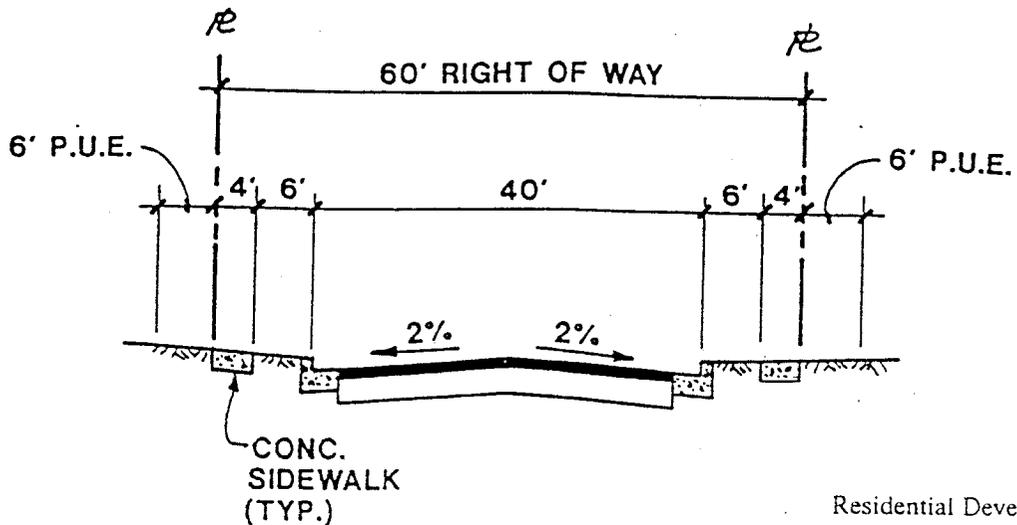
Exhibit 3-C

Residential Development

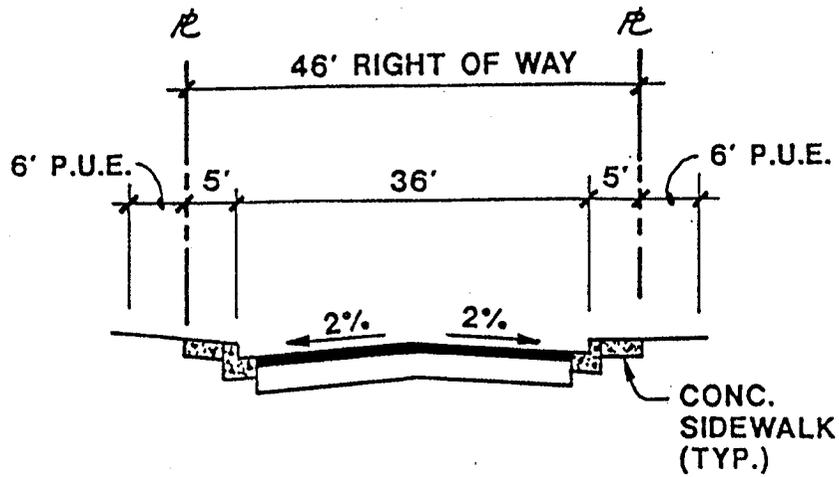
⑥ STREET G – RESIDENTIAL COLLECTOR WITH PARKWAY



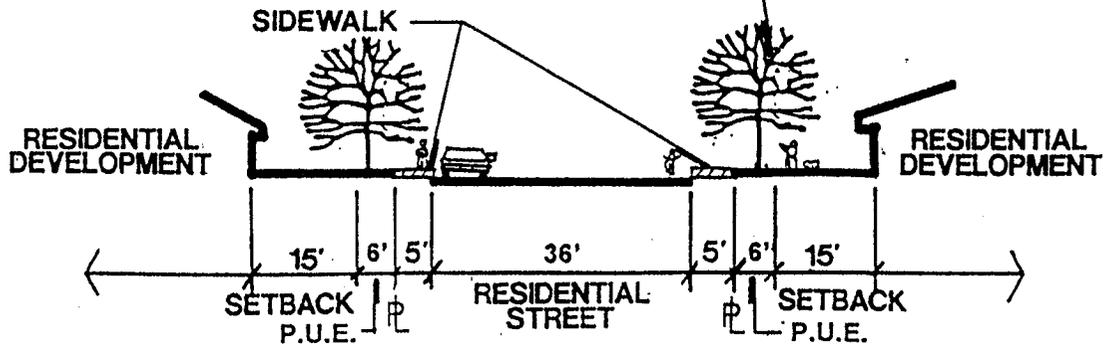
⑦ STREET G – RESIDENTIAL COLLECTOR



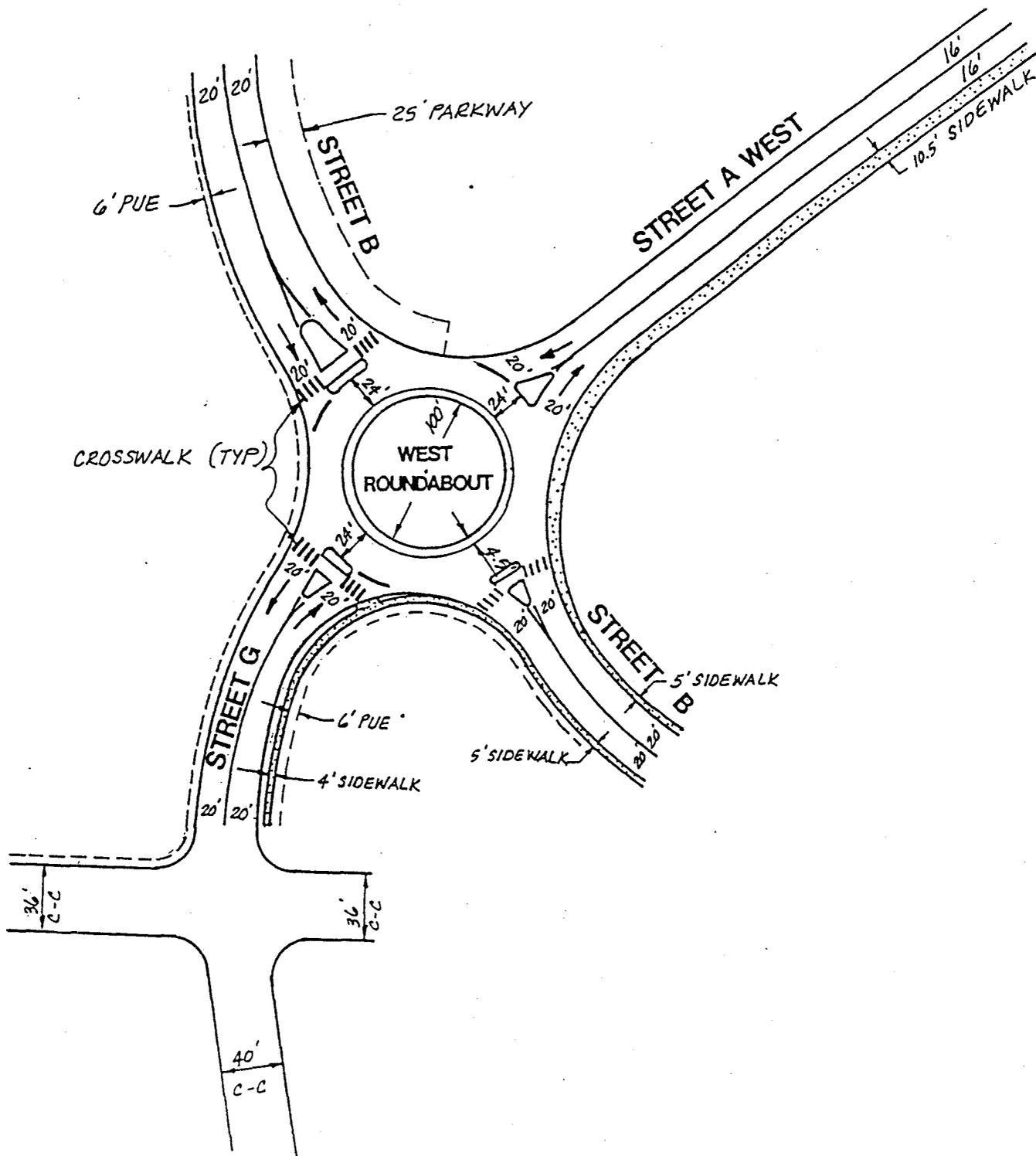
8 LOCAL RESIDENTIAL STREETS (TYPICAL)



EVENLY SPACED SMALL TO MEDIUM DECIDUOUS AND FLOWERING TREES SUCH AS—VARIES BY STREET



16 WEST ROUNDABOUT
PLAN VIEW



UNION PACIFIC R.O.W. SECTION

BUFFER SECTION

3.6.1 STREETScape PLANTING

1. Trees: the quantity of trees planted in the Streetscapes shall be one 24" box size tree for every 40' of street frontage or portion thereof.

All trees in the Streetscapes shall be planted a minimum of 3' (unless otherwise indicated) from structures and other improvements (curbs, sidewalks, walls & fences, underground trenches, etc.) and a maximum of 10' from back of sidewalk. Tree limbs extending into the street shall have a minimum ground clearance of 14'. Tree trunks shall be kept a minimum of 14' from all project street lights.

2. The Lawn Seed Variety for all Streetscapes shall be either Tall Fescue (*Festuca eliator*) or Dwarf Tall Fescue.

3.6.2 SUBDIVISION ENTRIES

- A. The residential development within the South of Route 92 Plan Area will be identified with formal monumentation elements at the intersection of Street A and Street B where a roundabout occurs. Corner cutoffs at both the northwest and southwest quadrants of the intersection will be utilized for this purpose. All entry improvements will be installed and maintained by the Master Developer.
- B. Entries into the individual subdivisions may be identified with monumentation and special planting. Elements proposed must be in keeping with the character and scale of the residential neighborhoods. All subdivision entry improvements must be approved by the Master Developer. Subdivision entries will be constructed by the individual builders and maintained, after subdivision buildout by the Home Owners Association (H.O.A.).

3.6.3 LANDSCAPE GRADING AND DRAINAGE

- A. All planting areas shall be graded no less than 2% to insure adequate drainage.
- B. The maximum slope for shrub and groundcover areas shall be 3:1 and the maximum slope for turf areas shall be 4:1.

3.6.4 OVERPASS AT RAILROAD TRACKS

- A. Approach ramp side slopes must be of soil no greater than 3:1 gradient, and landscaped in such a manner as to soften the visual impact of the structure. See cross-sections in Section 2.5.
- B. A 10' concrete walk adjacent to the curb to accommodate pedestrians and bicycles shall be constructed on one side of the overpass roadway.
- C. The portion of the overpass roadway directly over the railroad right-of-way shall be protected by a 6' high decorative iron fence, and shall include distinctive light standards. The exterior curb of the street and the interior of the sidewalk shall be protected by a 2 1/2' K-rail from both sides of the overpass down to finished grade, as illustrated in cross-sections in Section 2.5.

3.7 PARKS AND TRAILS

- A. Two neighborhood parks of 5 and 2.5 acres are strategically placed within the community to provide outdoor recreation opportunities to the residents. The larger park also creates a terminus to Street A and views toward the bay.

Tree massing at the larger park should occur along the park's north and south edges so as to frame the view of the westerly open space and distant bay. The edge shared with the roundabout should be more extensively landscaped. Interior portions of the site are best suited for large expanses of turf containing ball fields and open play areas. This park's location provides access to neighborhood trail system. Activities here will consist of both active and passive uses. Off-street parking and restroom facilities will be provided. This park will be maintained by H.A.R.D. (Hayward Area Recreation District).

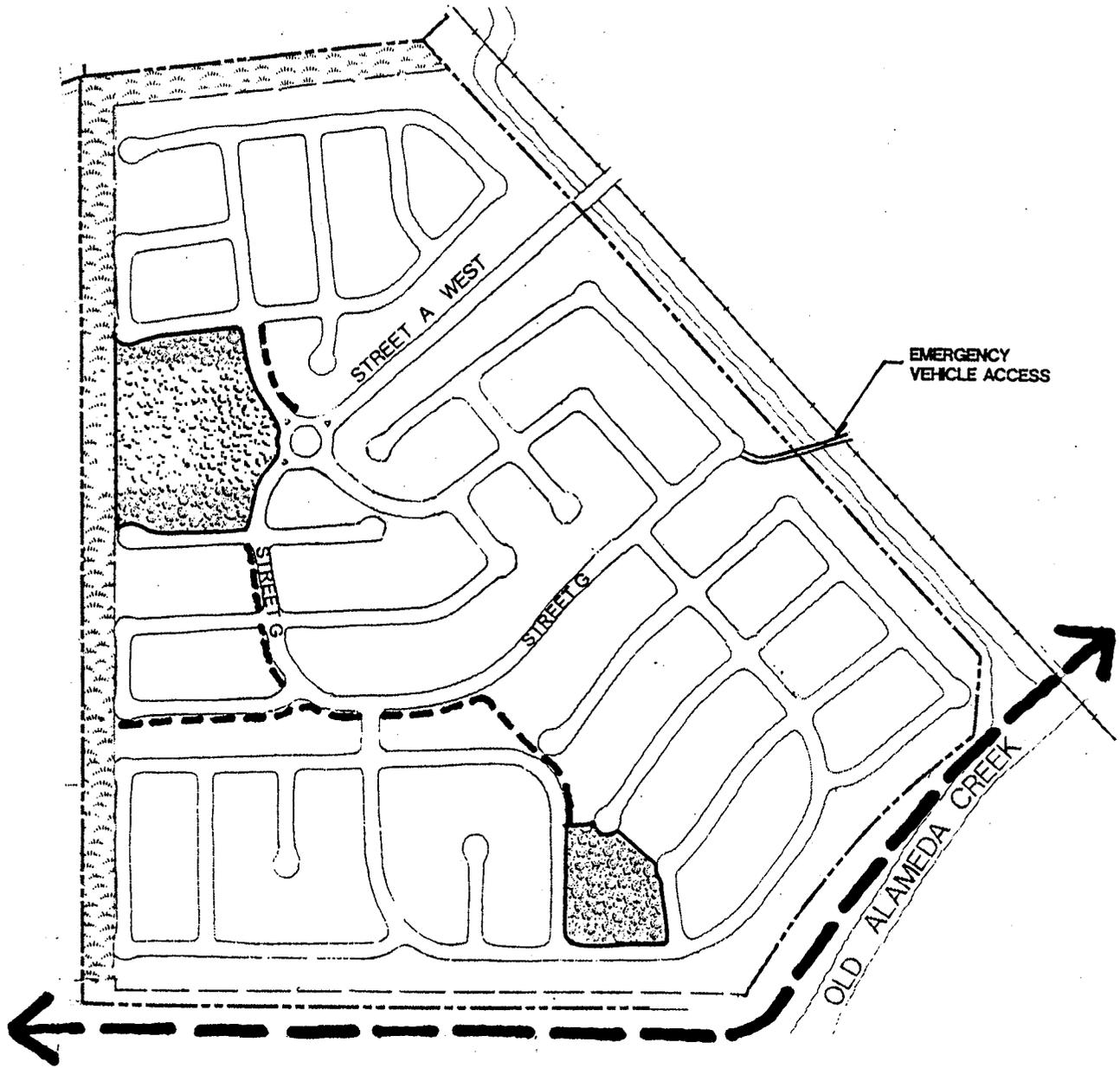
- B. On Hesperian Boulevard next to Street B East, an existing bike lane (Class II) runs along the west edge, a Class II bike lane runs along the east edge from Street A south beyond the city limit and an existing bike route (Class III) runs from Street A to Industrial Boulevard. North of Industrial Boulevard on Hesperian Boulevard is a proposed bike route as far as Baumberg Road. There is an existing route along Baumberg/Arden Road. Bicycle circulation within the plan area will be accommodated as Class III bike routes in the emergency parking lane on either edge of the major collector with signage and within regular traffic lanes on the local streets.
- C. An internal trail provides pedestrian and bicycle access through the community. A 6' wide sidewalk will run between the two neighborhood parks and will link the pedestrians and the overall project to the Bay Trail.

3.8 WETLAND EDGE/OPEN SPACE BUFFERS

Open space buffers are located at the north and west residential development edges on Oliver West. Their primary function is to provide protection of the designated habitat areas from errant trespass of people and domestic pets. These buffers will rely on broad waterways and wire fencing to perform their intended role. Buffer areas also provide significant aesthetic value. Once improved with graded slopes, waterways and landscaping buffers offer opportunities for neighborhood enrichment including park locations and vistas.

3.9 GRADING, DRAINAGE AND UTILITIES

- A. No excavation shall be made on and no sand, gravel, soil or material shall be removed from any parcel without approval of the Master Developer except in connection with the development of that site. Upon completion of such construction, depressions and holes shall be backfilled to grade, and disturbed ground shall be graded level and paved or landscaped.
- B. All subdivisions shall provide on-site storm drainage collection systems which are connected to the public storm drainage system. Sheet drainage across property lines and over driveways into streets is prohibited. Drainage must meet City standards for non-point discharge.



LEGEND



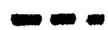
NEIGHBORHOOD PARKS



LANDSCAPE BUFFER ZONE



BAY TRAIL



6' NEIGHBORHOOD TRAIL



OLIVER WEST PARKS, TRAILS AND AMENITIES

Exhibit 3-D

Residential Development

- C. Fill material obtained from the construction of buffer areas and/or imported from other locations may be required to raise certain residential subdivisions above flood elevation.
- D. Due to the extremely flat nature of the site care will be necessary to provide for adequate sewer and storm drain grades to avoid excessive depths.

3.10 ARCHITECTURE

A. Architectural Character

A variety of traditional architectural expressions are envisioned for the residential section of the South of Route 92 Residential Plan Area. The resulting varied street scene should recall neighborhoods established over time. Builders are encouraged to offer products with elevations that are noticeably dissimilar from those of other builders. Roof overhangs, porches, varied front setbacks and entry courtyards will help to distinguish the separate neighborhoods.

B. Massing

Massing refers to the boxiness of a house. The intent is to mitigate boxiness and enliven the street scene through articulated building elevations; varied plate lines and roof forms, use of porches, courtyards, and color.

C. Front Porches

Porches or courtyards in front yards are required on 25% of the homes built by each builder. Porch design must be integrated to the architectural theme of the home and include a covered roof. Porches are considered part of the building footprint and must conform to front yard setback requirements.

D. Materials

All surface treatments or materials shall be designed to appear as an integral part of the design, and not merely applied. Material changes must occur at inside corners. Materials applied to any elevation shall turn the corner of a building a full 4' before terminating at a stucco pop-out providing an inside corner.

3.11 APPROVED PLANT MATERIALS

These Development Guidelines are not intended to identify specific plants but rather to suggest the shape and intended function of the hardscape.

The use of specific plants will be subject to direction provided in the following city publications and review and approval by the City Landscape Architect:

1. STREET TREES, Section 7-2.50 through Section 7-2.65
2. STREET TREE PLANTING SPECIFICATIONS, June 15, 1993, Drawing no. SD-122
3. LANDSCAPE DESIGN CHECKLIST, pages 1-6

4. OFFICIAL STREET TREE LIST, as prepared by the City Landscape Maintenance Manager.
5. HAYWARD LANDSCAPE BEAUTIFICATION PLAN, Oct. 1987, prepared by POD, Inc.
6. HAYWARD WATER EFFICIENT LANDSCAPE ORDINANCE

All of the above documents are on file and available in the Offices of the City Clerk and the Department of Public Works. For additional information please contact the City.

SECTION 4 - SPORTS PARK

4.0 SPORTS PARK

4.1 PLANNING CONCEPT

Development of a 25 acre parcel as a public Sports Park will provide both residents and workers in the South of Route 92 Plan Area and other Hayward residents with a variety of recreational opportunities not found in the area. A number of lighted game courts and ball fields will be available for informal as well as league type activities. Areas will be included for off-street parking, a concession building, shade structures, and restrooms. The landscape concept and requirements for maintenance will be determined by the City of Hayward and the Hayward Area Recreation District.

SECTION 5 - WALLS AND
FENCING

5.0 WALLS AND FENCING

5.1 BUSINESS PARK/LIGHT MANUFACTURING WALLS AND FENCING

The use of fencing and walls between the front property line and the building should be minimized. Any walls or fencing should be consistent with the architectural character of the buildings. Barbed wire and chain link fencing is not permitted. Long walls should have periodic visual breaks such as indentations, rhythmic projecting columns, raised planters or staggering of the wall. See Exhibit 5-A.

5.2 RESIDENTIAL WALLS AND FENCING

The use of fencing and walls between the front property line and the building should be minimized. Any walls or fencing should be consistent with the architectural character of the buildings. Barbed wire and chain link fencing are not permitted. Long walls should have periodic visual breaks such as indentations, rhythmic projecting columns, raised planters or staggering of the wall. Walls are required between abutting residential areas and business park/light manufacturing uses and where side or rear yards abut habitat or wetlands area. See Exhibit 5-A.

The use of fencing and walls should be consistent with the architectural character of the homes. Barbed wire is not permitted. Long walls should have periodic visual breaks such as indentations, rhythmic projecting columns, or pilasters. No residential walls or fences shall exceed 6' in height.

A. Solid masonry walls or wood fencing are recommended for :

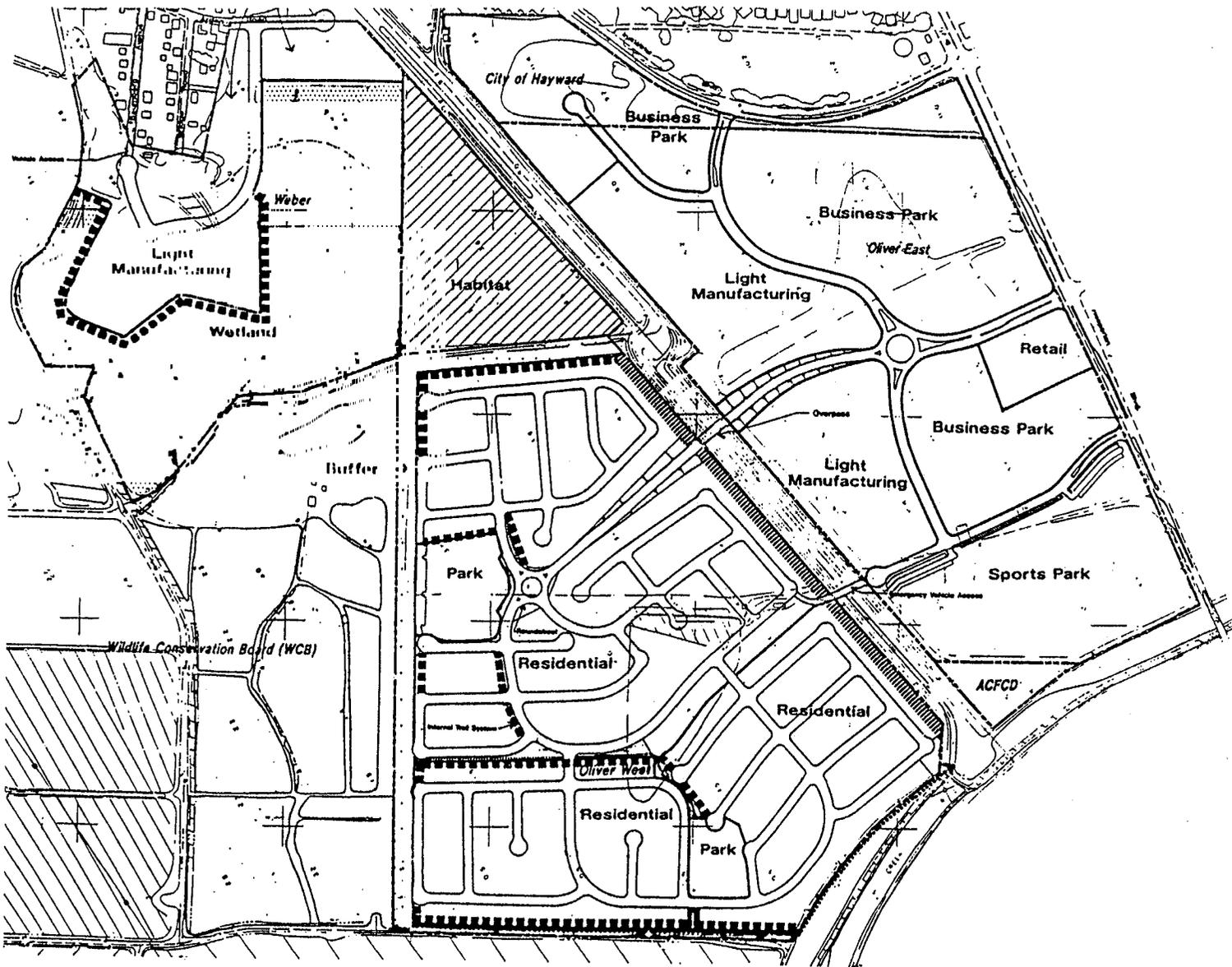
1. Screening rear and side yard from public view. Rear yards abutting a public street should be screened with a masonry wall. Walls should be set back from the sidewalk a minimum of 10' and setback shall be landscaped with shrubs or vines. Masonry walls are prescribed for lots with a rear wall along Old Alameda Creek.
2. Providing adequate levels of sound attenuation for homes adjacent to the railroad R.O.W. (may exceed 6' in height if combined with berm that is less than 4' in height.)
3. Preventing trespass and access of domestic pets into adjacent habitat areas.

B. Wire mesh or wrought iron fencing is recommended for:

1. Front and side yard applications where fence height does not exceed 4' in height.

C. All residential fences and walls shall be constructed by the residential builders and approved by the City's Planning Director

D. Side yard walls or fences within the front yard setback should not exceed 4'.



LEGEND



SIDE OR REAR YARD WALL: MAXIMUM HEIGHT: 6'



SOUNDWALL HEIGHT: 10' (WALL AND BERM)

PLAN AREA WALLS AND FENCING KEY MAP

SECTION 6 - LIGHTING

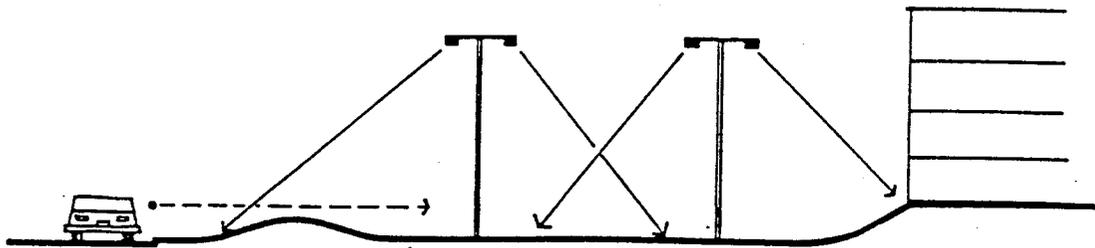
6.0 LIGHTING

6.1 OBJECTIVES

1. To have on-site lighting contribute to site security, and the safe and efficient use of a parcel
2. To have on-site lighting complement and reinforce the architecture and parcel design character
3. To have on-site parking area light fixtures and illumination levels consistent throughout the South of Route 92 Business Park and light manufacturing areas
4. To prevent on-site lighting from casting glare onto adjacent parcels and streets
5. To encourage lighting design that is in conformance with energy saving guidelines.

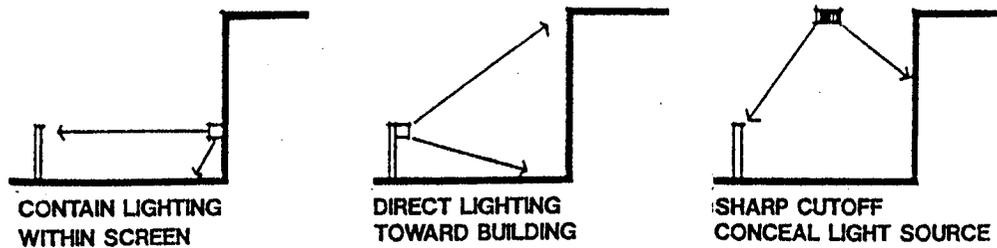
6.2 GUIDELINES

- A. All individual parcel lighting over 10' in height and potentially visible from an adjacent street shall be indirect or incorporate a full cut-off shield type fixture.

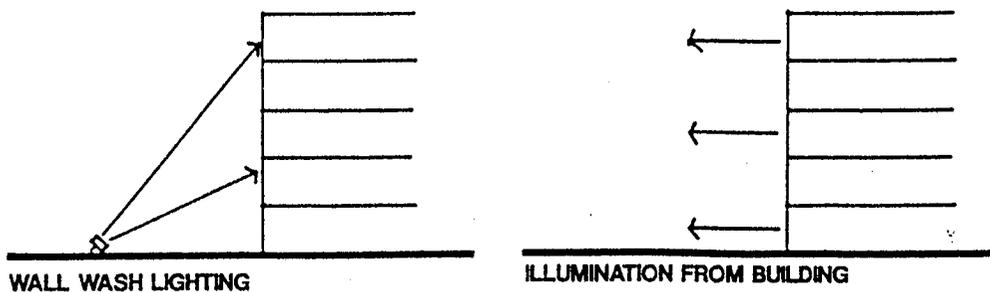


MOONLIGHT EFFECT-CONCEALED LIGHT SOURCE

- B. Parking areas, access drives, and internal vehicular circulation areas:
- a) As a minimum requirement, site lighting on all parcels shall provide side and rear yard pole lighting with a maximum 25' fixture height to provide the minimum standard site lighting set forth herein.
 - b) Poles shall be set at least 2' behind curb with footing flush with grade in planting areas and walks. Provide a three foot high cylindrical concrete base within paved parking areas and reduce pole length to compensate for height of base and achieve uniform mounting height.
 - c) The light source shall be a 400 watt high pressure sodium lamp. The parking area illumination level shall achieve a uniformity ratio of 3:1 (average to minimum) with a minimum of 1' candle of light across the entire surface of the parking area.
- C. Service area and outdoor storage area lighting shall be contained within the area boundaries and enclosure walls. No light spillover should occur outside the parcels. The light source shall not be visible from the street.

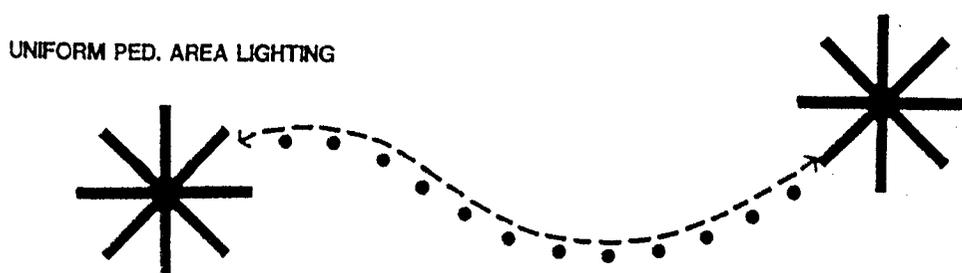


D. Building illumination and architectural lighting shall be indirect in character (no light source visible). Indirect wall lighting or “wall washing”, overhead down lighting, or interior illumination which spills outside is encouraged. Architectural lighting should articulate and animate the particular building design as well as provide the required functional lighting for safety and clarity of pedestrian movement.



E. Pedestrian walk and area lighting is segmented into two zones.

- a) The first zone is pedestrian area lighting. This is outdoor pedestrian use areas such as courtyard, entryway, etc. Pedestrian area lighting shall achieve a uniformity ratio of 3.5:1 average to minimum, with an average illumination of .60' candles and a minimum of .18' candles.
- b) The second zone is pedestrian walk lighting where point to point lighting is acceptable with no specific illumination levels required. The main emphasis in this zone should be to clearly identify and illuminate the pedestrian walkway and direction of travel.



SECTION 7 - SIGNAGE

7.0 SIGNAGE

The function and purpose of a Sign Program is to establish a unified scheme of signage for the South of Route 92 Plan Area. The goal of the Sign Program shall be to harmonize the appearance of affected signs, in a manner allowing creativity in design and commercial/retail identification, while complying with the requirements set forth in Section 7.0 of these Development Guidelines. Each Sign Program shall indicate on a plan view diagram the sign types, height, locations, quantity, sizes, materials, color, and illumination of each sign. An individual Sign Program is required for each individual parcel as part of the South of Route 92 Plan Area Master Developer's Preliminary Plan Review. See Section 8.0.

7.1 OBJECTIVES

- A. To regulate the type, size, number, design, and placement of signs, in a manner that will enhance the aesthetic appearance of the South of Route 92 Plan Area in a manner consistent with the necessity for business entities within the South of Route 92 Plan Area to identify the locations of their businesses to visitors and to the general public.
- B. To restrict signs displayed for purposes other than business identification.
- C. To provide for informational, directional, and traffic control signage necessary to safe, convenient and efficient vehicular and pedestrian traffic.
- D. To establish minimum standards for the durability, safety, quality, and type of materials used in construction of signs with the South of Route 92 Plan Area.
- E. To regulate temporary signage identifying future building occupants, design consultants, construction contractors, lending institutions, sale or leasing agents, and the types of building space or buildings to be constructed.

7.2 PROHIBITED SIGNS

No person shall erect, maintain, display, relocate or place any sign of the following types within the South of Route 92 Plan Area:

1. Animated signs
2. Audible signs
3. Flashing signs
4. Internally illuminated signs
5. Metal-framed box signs
6. Moving signs
7. Off-site signs
8. Portable signs (unless temporarily associated with residential project openings)
9. Roof-mounted signs

10. Signs with exposed transformers

11. Window signs located above the first story of a multiple-story building.

7.3 SIGN DESIGN AND APPROVALS

Each proposed Sign Program must first be approved by the Master Developer. Once approved, the Sign Program must be approved by the City of Hayward. See Section 8.0 for the City's review process.

All signs are to be designed, fabricated, and erected per the City of Hayward's Sign Ordinance; currently under amendment, Section 10, article 7, of the Hayward Municipal Code.